

PIKE & PINE STREETScape CONCEPTUAL DESIGN



December 2008

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Zimmer Gunsul Frasca Architects LLP

City of Seattle Department of Planning and Development

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Introduction

The City of Seattle is committed to improving streetscapes in downtown and intends to build on the success of the Third Avenue improvements implemented in November 2007.

As part of this effort, the city is funding several conceptual streetscape design documents for use by stakeholders. The Pike & Pine Conceptual Design plan from First through Fourth Avenues is the first of these documents. These designs, reviewed in advance by city departments, will provide greater predictability for stakeholders when making investments in city rights-of-way. This plan was prepared by Zimmer Gunsul Frasca Architects LLP under a contract with the City of Seattle, Department of Planning and Development.

The study area for this plan is Pike Street and Pine Street between First and Fourth Avenues. This plan was coordinated with another consultant team hired by Seattle Department of Transportation (SDOT) to look at improvements to Pike and Pine from First through Eighth Avenues and with the Westlake Transit Hub.

Prior to design, the study team examined previous plans for the study area, including the 1995 Pine Street Advisory Task Force and the 1997 Pike Street Improvement Project, as well as the legacy of past improvement projects including the Pike Place Market, Pike Local Improvement District (LID), Westlake Park, and the Pine Street Improvements. These improvements addressed pavement, street lighting, street trees, tree grates and planters. The intent of the Pike and Pine planning effort summarized in this document has been to preserve the best of past plans and improvement projects, and incorporate them into this plan.

The scope of this planning effort included siting of curblines, location of utilities and areaways, drafting a set of comprehensive streetscape design guidelines, provision of sustainable design options, and creation of conceptual design documents and a PowerPoint presentation.

City goals for the plan included a comprehensive streetscape conceptual design plan for use by stakeholders, pre-

approval of these plans by city departments, engagement of stakeholders in the process, and coordination with the larger project addressing Pike and Pine Streets from First through Eighth Avenues.

The planning process included a series of meetings with city departments and stakeholders in March, April and May, 2008, to review the initial vision and the preliminary and final conceptual plans for this document.

How to use the Conceptual Design Plans

The Conceptual Plans represent a vision for Pike and Pine Streets from First through Fourth Avenues. Portions of the plans will be implemented over time by individual property owners as parcels are redeveloped. Projects will be reviewed in the context of these conceptual design plans.

The conceptual plans also provide a menu of options for applicants who are not redeveloping property, but wish to beautify the streetscape abutting their property within the area covered by the plan. When applicants are improving the streetscape only, the property owner can opt to implement selected elements of the plan rather than the entire plan. For maximum impact, however, the City of Seattle strongly recommends that property owners implement the complete plan. In addition, multiple property owners may wish to work together to complete an entire block. A collaborative approach can lead to construction savings and guarantees a much greater visual impact.

If a property owner wishes to improve sections of streetscape that extend past his or her property line, permission must be obtained from owners of property abutting all streetscapes proposed for improvement. Maintenance and liability/indemnification agreements must be established in such cases.

Urban Context

Following are observed characteristics of the urban context of Pike and Pine Streets:

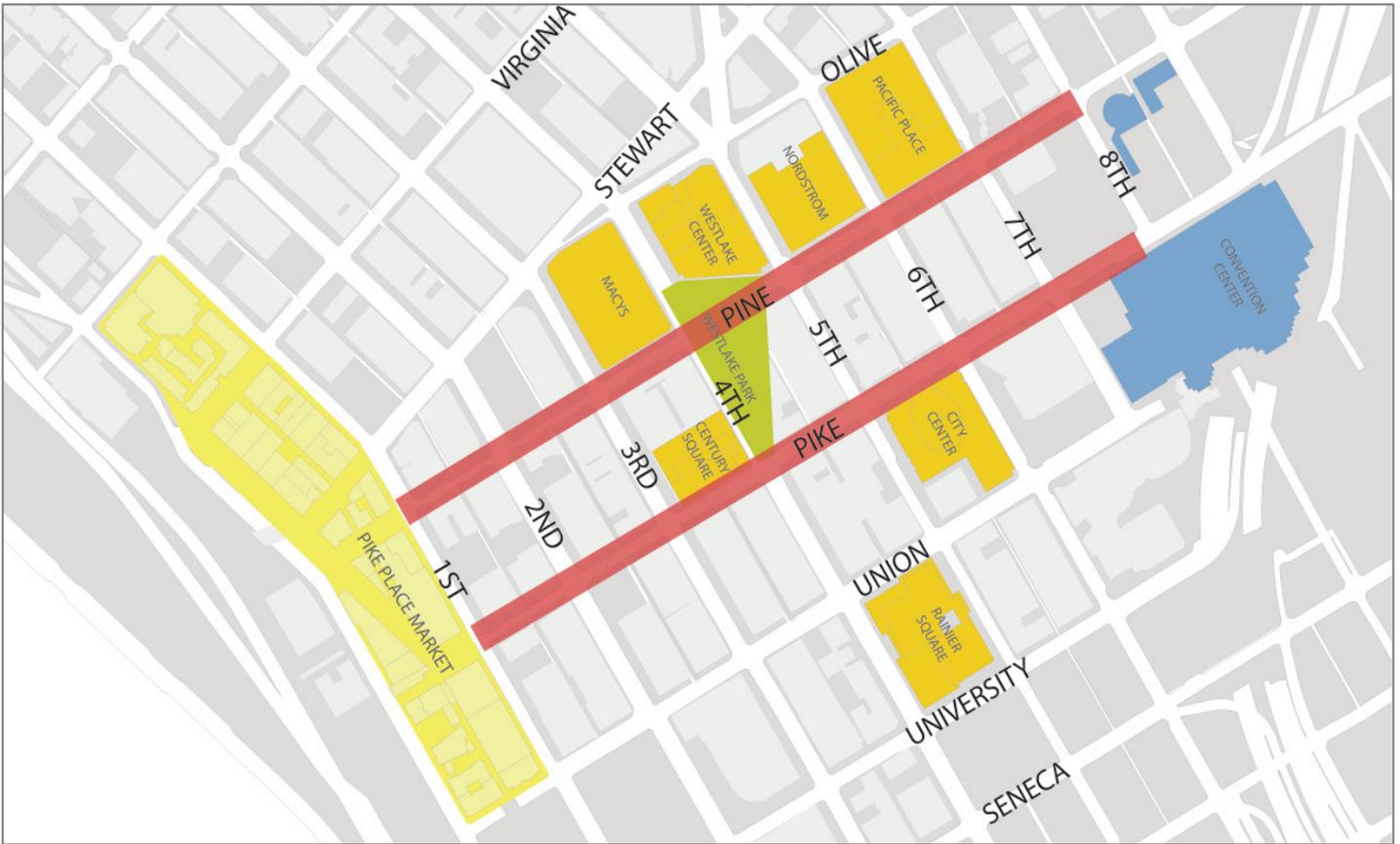
- Prime streets in the retail core, tying together three of the city's premier places – Pike Place Market, Westlake Park and the Convention Center
- Front door to Seattle's department stores and multi-level shopping malls
- A promenade through the retail core, or "heart of Seattle", to the Market, "Seattle's soul"
- The short ends of long blocks with alleys—more potential for corner stores and street corner socialization
- Level ground encourages north/south and east/west pedestrian circulation
- Blocks between First and Second on Pike and Pine are the "front porch of the Market"
- Pike and Pine streets together function as a one way couplet (Pike eastbound and Pine westbound) for general purpose traffic and as a key corridor for transit and cyclists
- Transit plays an important role bringing shoppers, workers, and residents to the area and activating the street level
- Transit, pedestrian and bicycle emphasis in the retail core is supported by First and Third Avenue traffic restrictions
- Free right turns on red increases potential for conflicts between pedestrians and vehicles
- North sides of the streets receive solar exposure conducive to sidewalk cafés
- The transit stop on Pike Street west of Fourth Avenue experiences pedestrian congestion, view blockage and security concerns (drug dealing, panhandling, etc.)
- The transit island on Pine between Third and Fourth isolates riders and reduces the sidewalk width on the south side of the street
- The sitting ledge near the transit tunnel entrance attracts behaviors such as panhandling
- The transit layover zone on the south side of Pike Street between Second and Third Avenues obstruct views and reduces streetscape activation
- The Pike Local Improvement District (LID) and Pine improvements left a positive legacy of street trees and ornamental lighting
- Pike Street's paving and street furniture have deteriorated
- Selected Pike Street trees are in poor condition
- Some ground floor uses do not enhance the pedestrian environment, especially on Pike and Pine Streets between First and Second
- New development and renovations are planned on Pike and Pine including: Second and Pike; Joshua Green Building; Liberty Building; Century Square; Macy's Garage; and Second and Pine



Visual terminus of Market sign on Pike Street



Visual terminus of Convention Center canopy on Pike Street



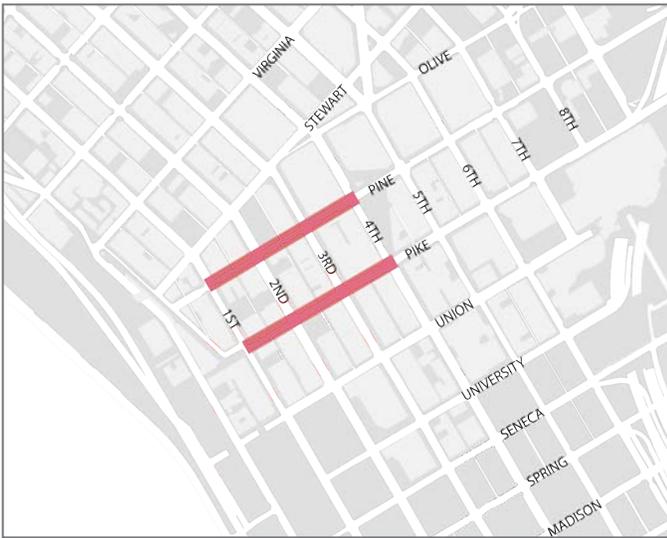
Heart of downtown Seattle's Retail Core



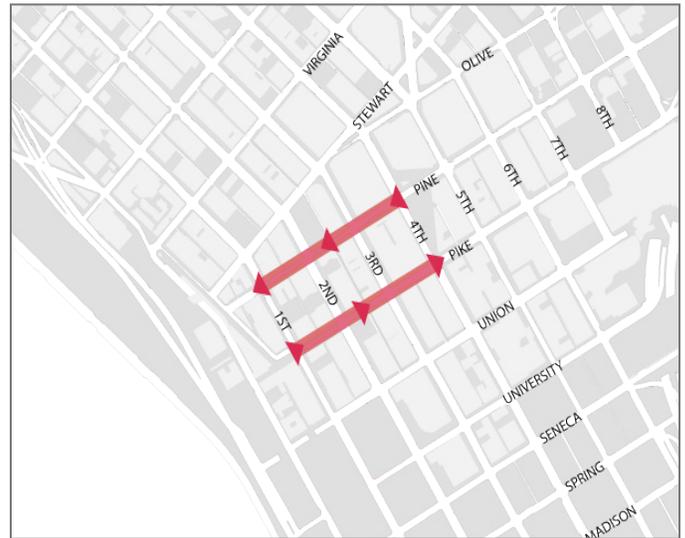
Historic Landmarks include:

- Ben Bridge, Carroll's and Century Square street clocks
- Bon Marché (Macy's)
- Joshua Green, Liggett, Olympic Tower, Seaboard, Doyle and Eitell Buildings
- The Pike Place Market

Urban Context



Pike & Pine Streetscape Conceptual Design Area



One way couplet (Pike St eastbound & Pine St westbound)

Past Projects:



Pike LID sandstone and concrete paving



Pike Place Market brick paving



Pine Improvements scored concrete paving



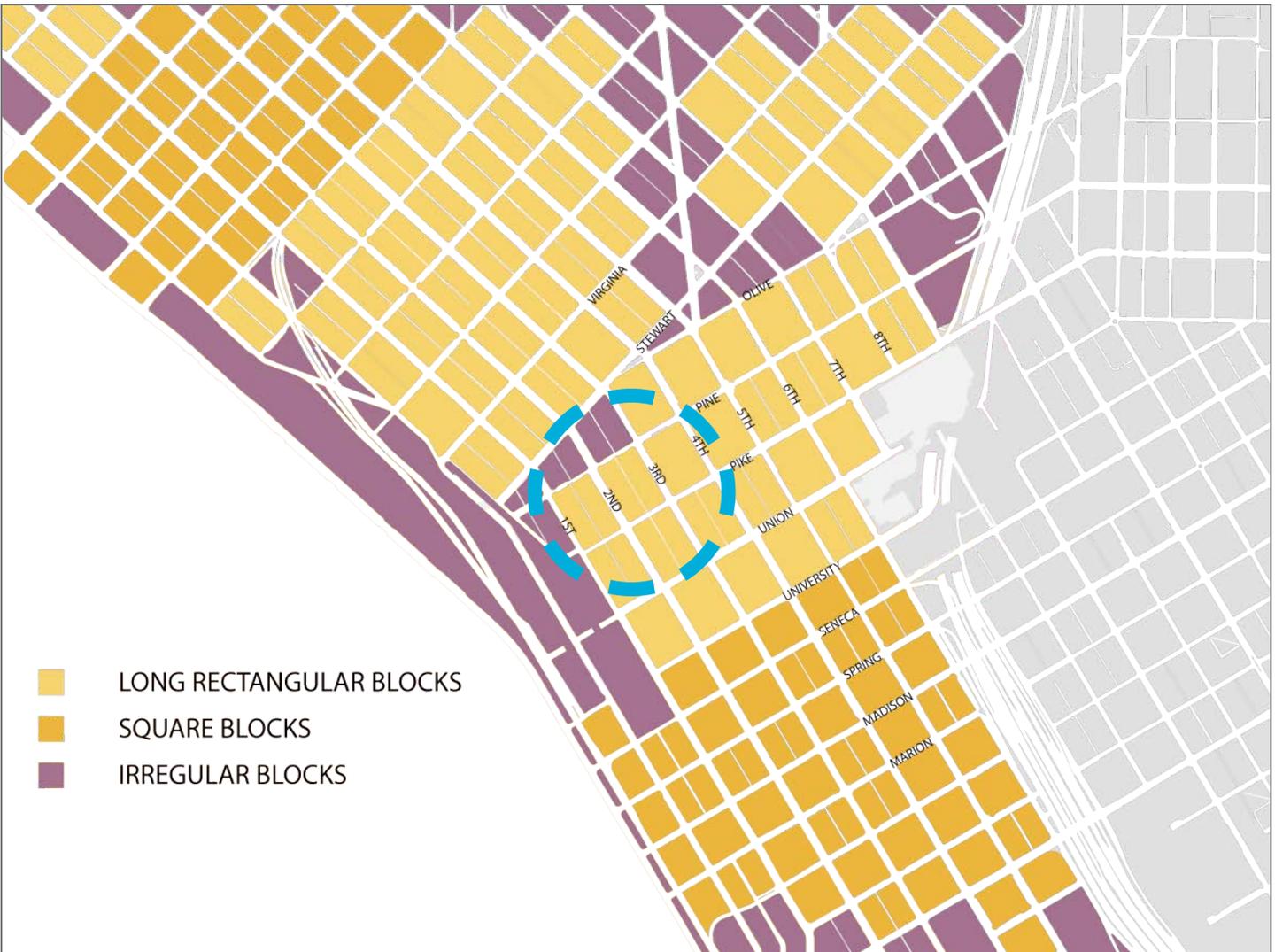
Westlake Park decorative granite paving



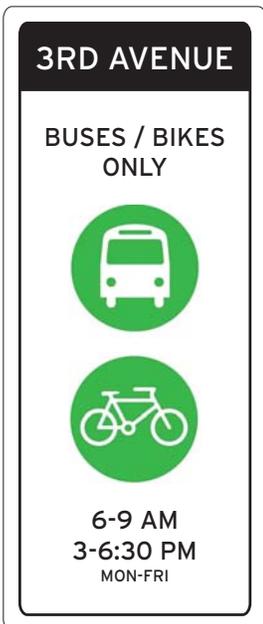
Free right turns can lead to conflicts between vehicles and crossing pedestrians



Level ground encourages east/west and north/south pedestrian circulation



Long blocks with short ends and alleys



Paving from Pike LID project in poor condition

Transit/bicycle emphasis is supported by vehicular access restrictions at First and Third Streets

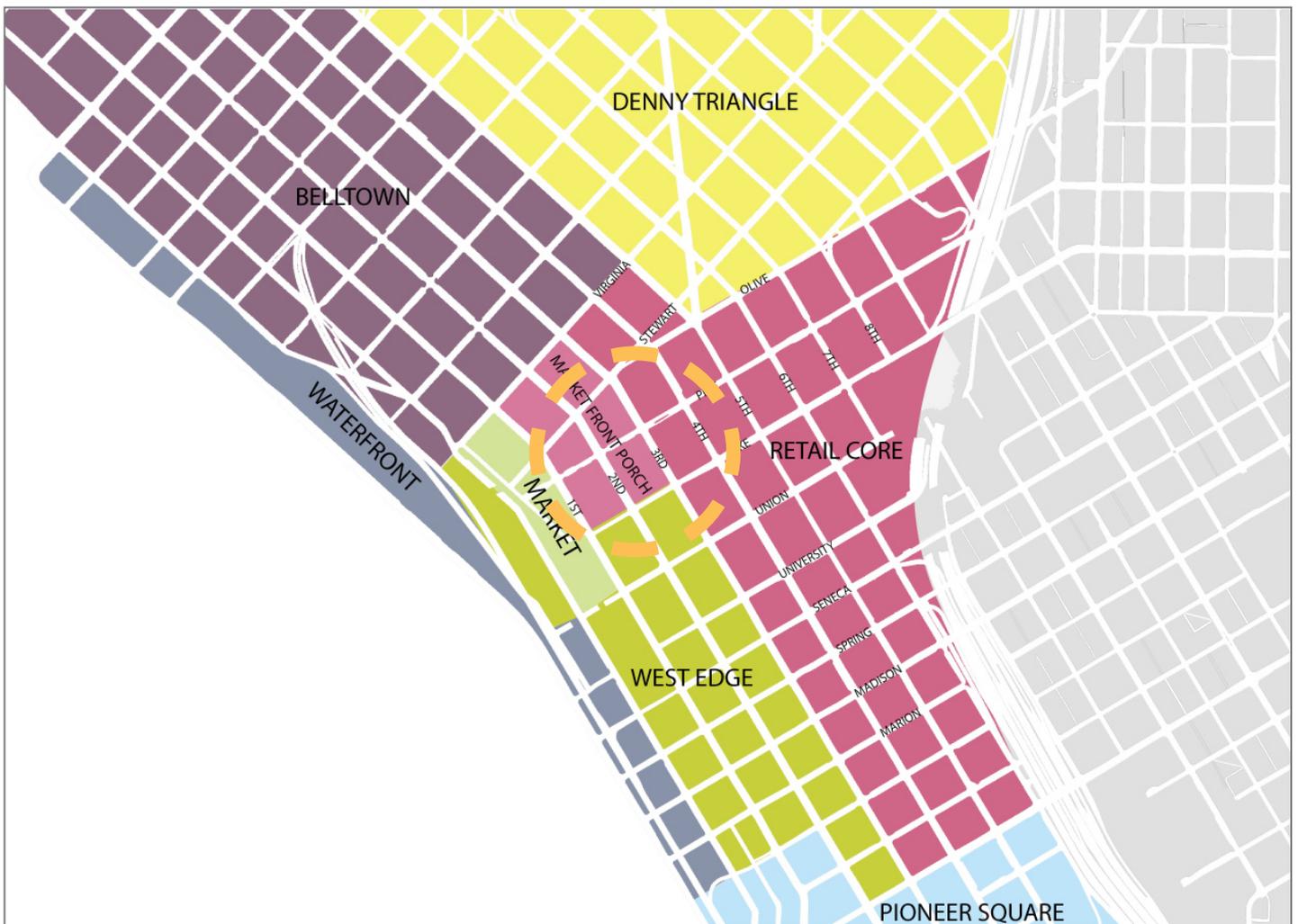
Urban Context



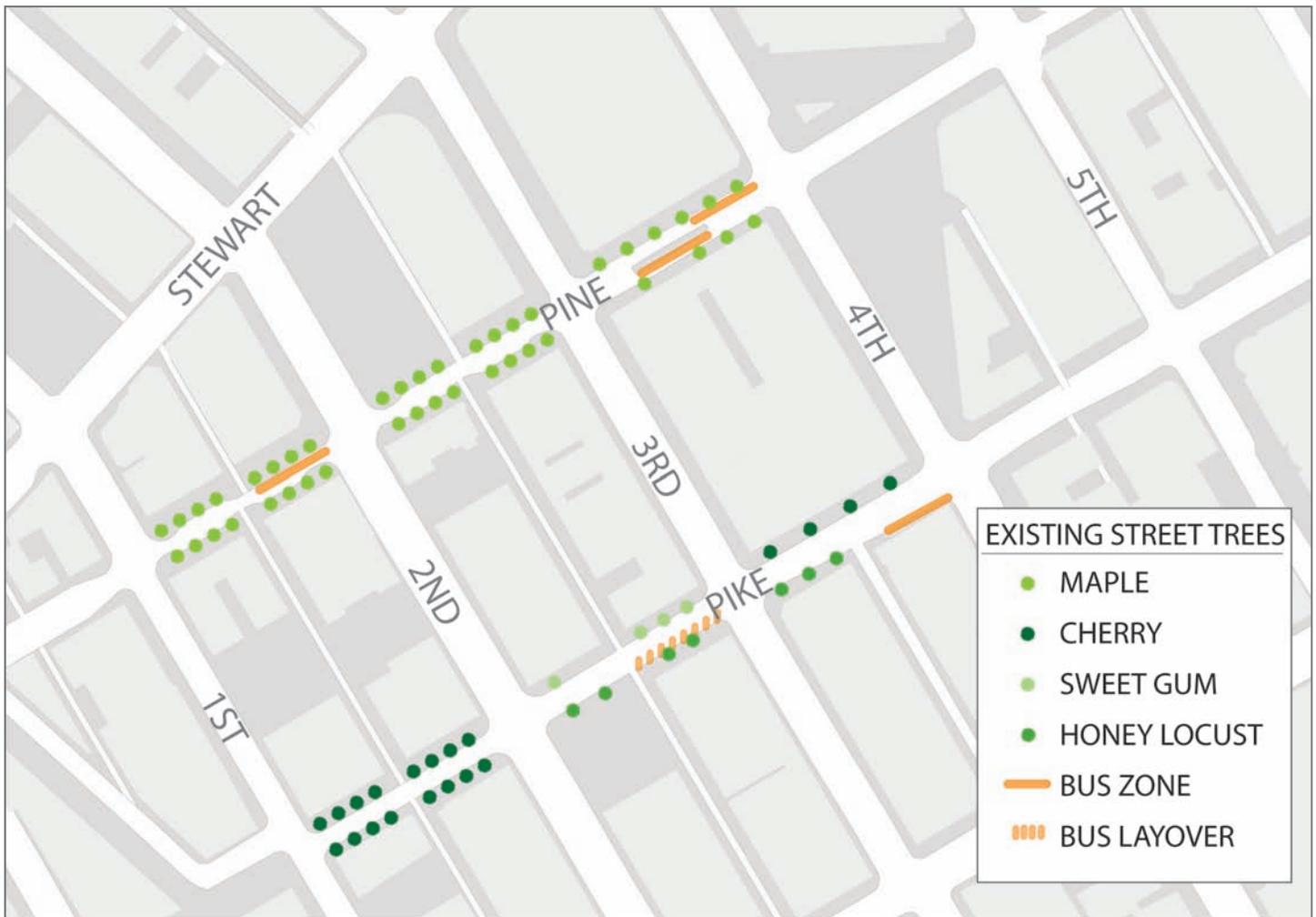
Pike Street - Front door to the market



Pine Street - Side door to the market



Downtown Districts



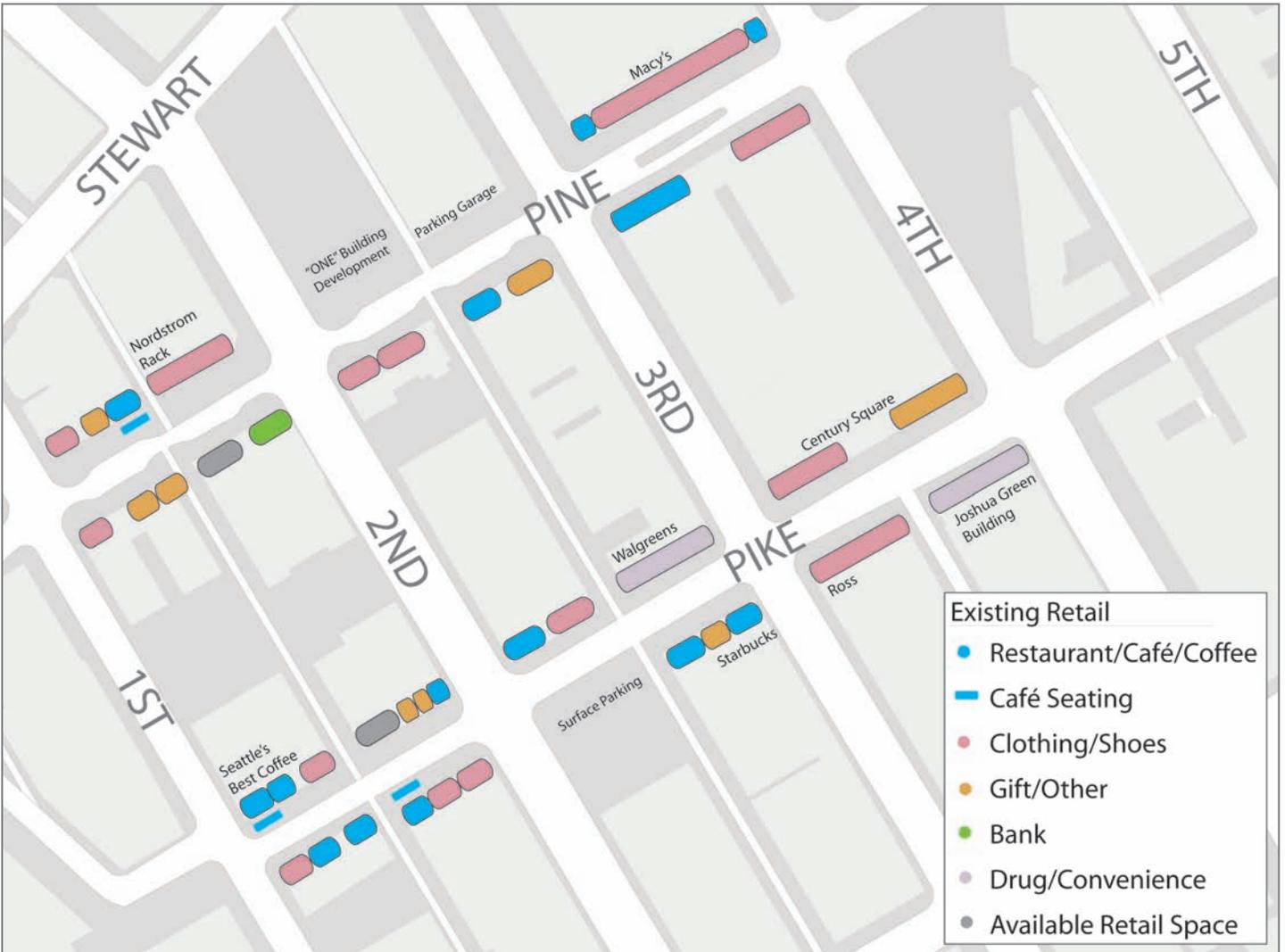
Existing street trees and transit stops



Ample street trees on the 100 block of Pike Street



Need for streetscape amenities and animation on Pine Street



Existing Retail



Pike and Pine sun survey at noon, June 21; solar access is generally good



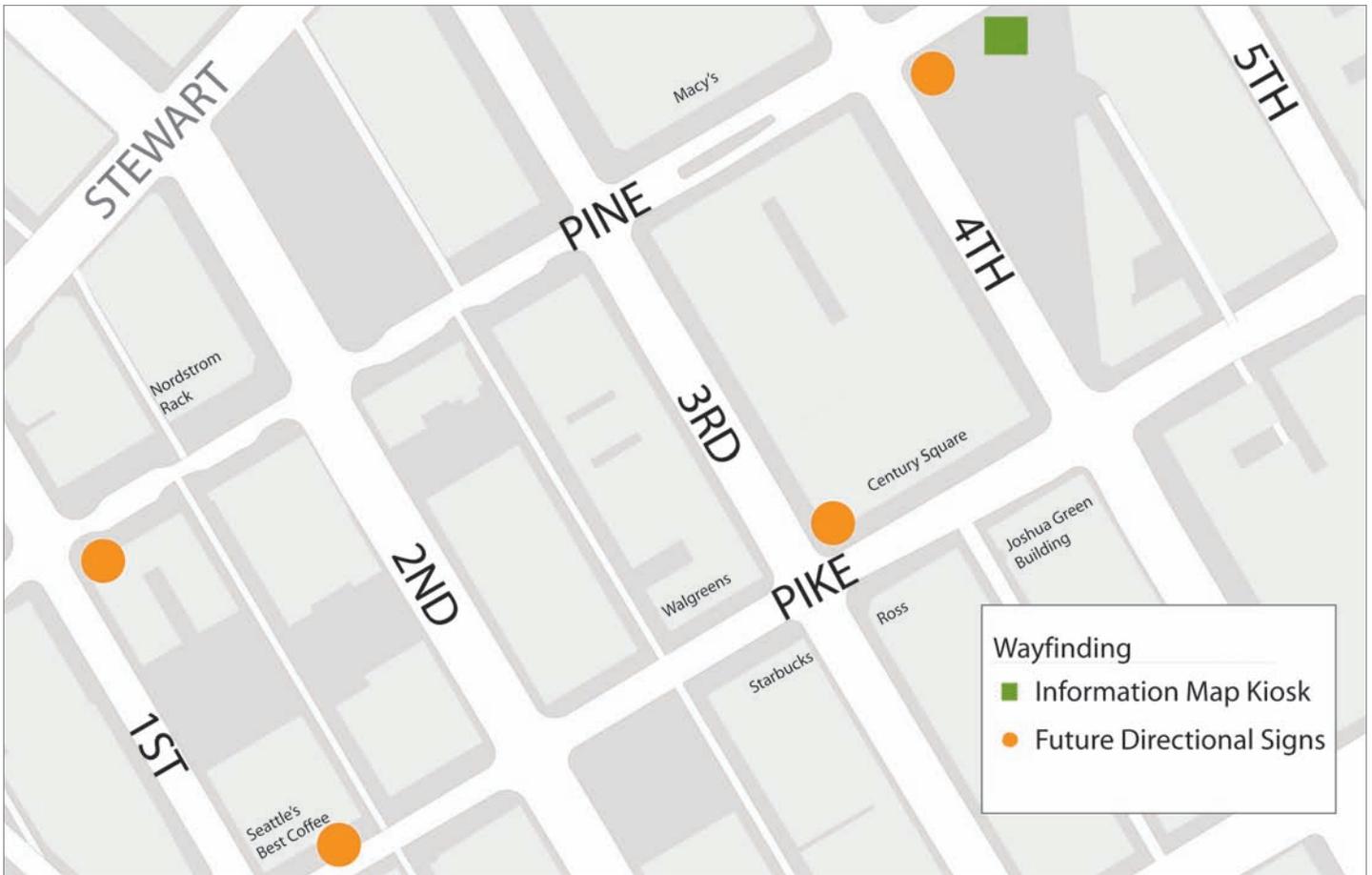
Potential Development



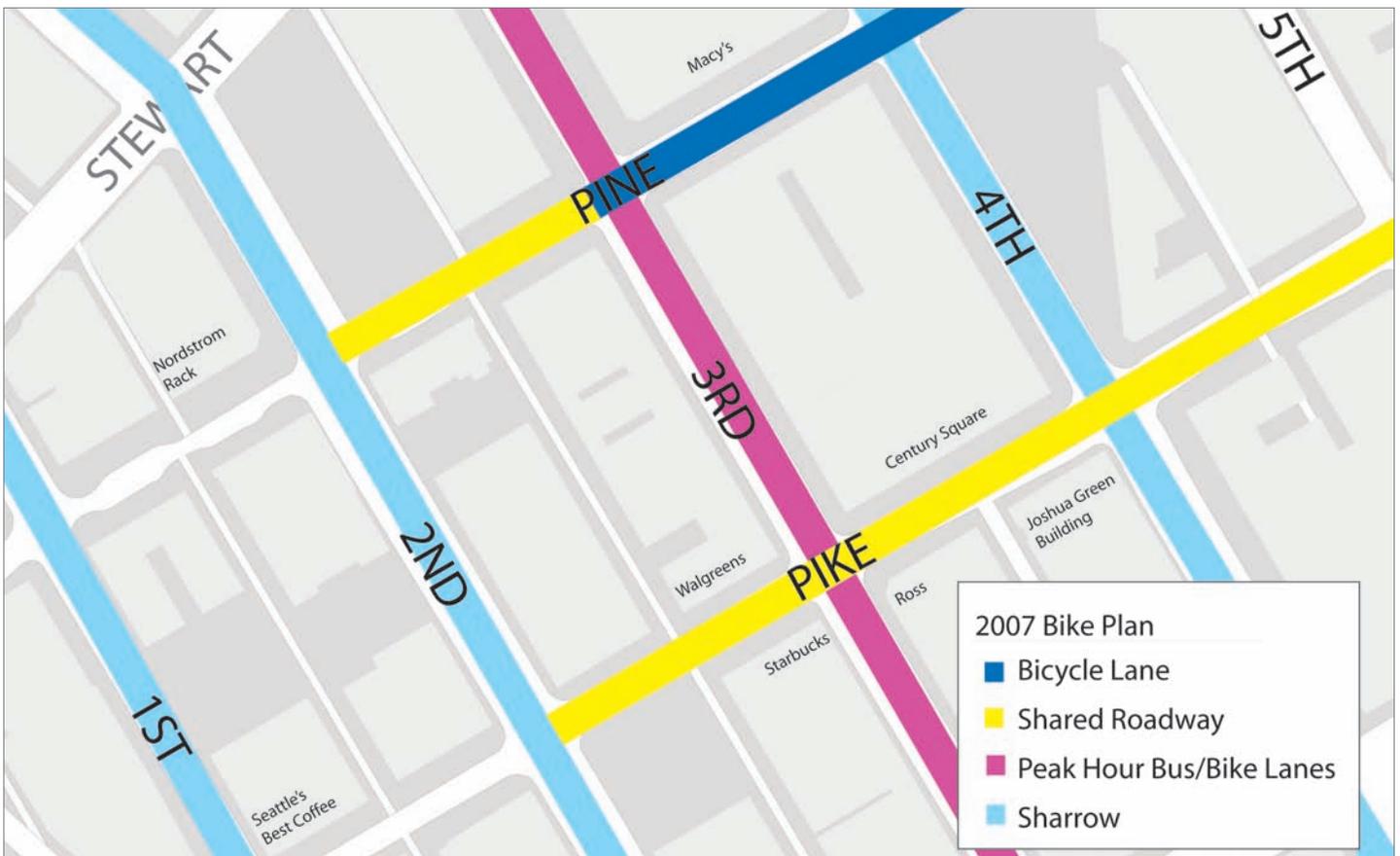
Existing table and chair seating



Existing sidewalk café seating



Wayfinding



2007 Bike Plan



Areaways (building extensions under the sidewalks) limiting planting



Transit shelter at Joshua Green Building reduces storefront visibility

Design Goals & Objectives

While preserving the best from the past, the Design Goals & Objectives aim to enhance the Pike & Pine retail core through the following actions:

Enhance Pike & Pine Streets as the heart of the retail core and create a promenade connecting three premier places: Pike Place Market; Westlake Park and the Convention Center:

- Establish the block between First and Second Avenues on Pike and Pine streets as the front porch and gateway to the market
- Provide more greenery with surface plantings and hanging flower baskets
- Define streetscape elements of continuity and differentiation

Encourage the stewardship of fronting properties:

- Integrate transit facilities into building frontages and remove transit shelters
- Encourage the presence of small, local retail shops and restaurant and cafés that provide outdoor seating
- Foster public/private partnerships for streetscape maintenance and management programs, street level leasing and storefront enhancements

Preserve the best from the past:

- Current curblines
- One-way traffic on Pike and Pine Streets
- Selected distinctive paving: brick on First Avenue at the Market, granite at Westlake Park, special pavement at major retailers
- Healthy street trees
- Ornamental pedestrian lighting
- Sidewalk café seating
- Historic buildings and street clocks



Opportunity for more greenery at Century Square



Art tree grates add interest, but limit surface landscaping



Potential for hanging flower baskets from storefronts

Design Goals & Objectives

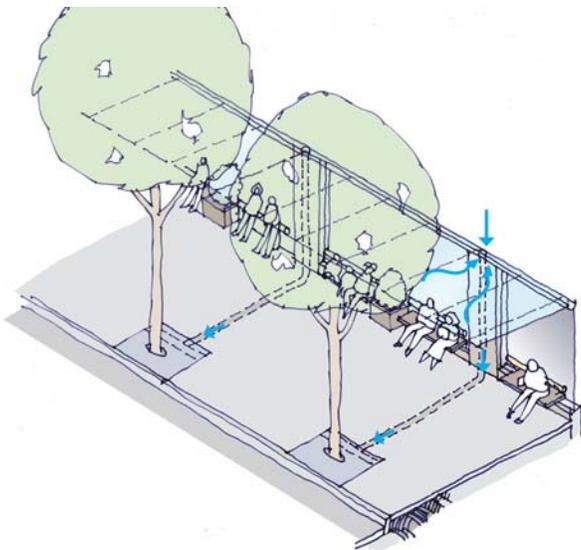
- Westlake Park distinctive street furniture elements
- Character and local flavor
- Sidewalk security, management and maintenance programs

Reinforce the Heart of the Retail Core:

- Develop Pike and Pine Streets and their intersections with north/south avenues as the heart of the city's retail core
- Enhance the quality and greenery of the streetscape by expanding surface planting areas under street trees
- Encourage high quality storefronts, retail signage and commercial lighting
- Encourage seasonal programming and special features (holiday tree lighting, public art, hanging flower baskets on poles and storefronts)
- Integrate bus waiting zones into building frontages to increase storefront visibility and eliminate bus islands and shelters

Develop a front porch and gateway to the Market:

- Respect views to the Pike Place Market and provide character to the blocks between First and Second consistent with the flavor of the Market



Opportunity to accommodate transit in building frontages and capture stormwater to irrigate landscaping



Pedestrian-oriented retail signage perpendicular to the sidewalk and integrated with glass canopies is desirable

Create a promenade connecting the Convention Center, Westlake Park and the Market:

- Enhance aesthetics and the sense of civility and safety in the corridor
- Provide elements of continuity (consistent 2' x 2' scored concrete paving, wayfinding and interpretive signage, etc.)
- Replace deteriorated street furniture, paving and street trees
- Provide bronze inlaid street names at intersections per city standards

Integrate with Westlake transit hub:

- Reinforce wayfinding at transit tunnel entries and enhance transit information systems

Maintain Transportation System Functionality:

- Preserve transit stops and encourage alternative modes of transportation
- Retain parking pay stations and on-street parking

Offer sustainable design options:

- Retrofit street lighting to reduce light pollution and increase energy efficiency
- Encourage glass canopies that capture and direct stormwater to irrigate landscaping
- Encourage the use of permeable paving between surface planting beds to promote irrigation of landscape and stormwater infiltration

Design Goals & Objectives

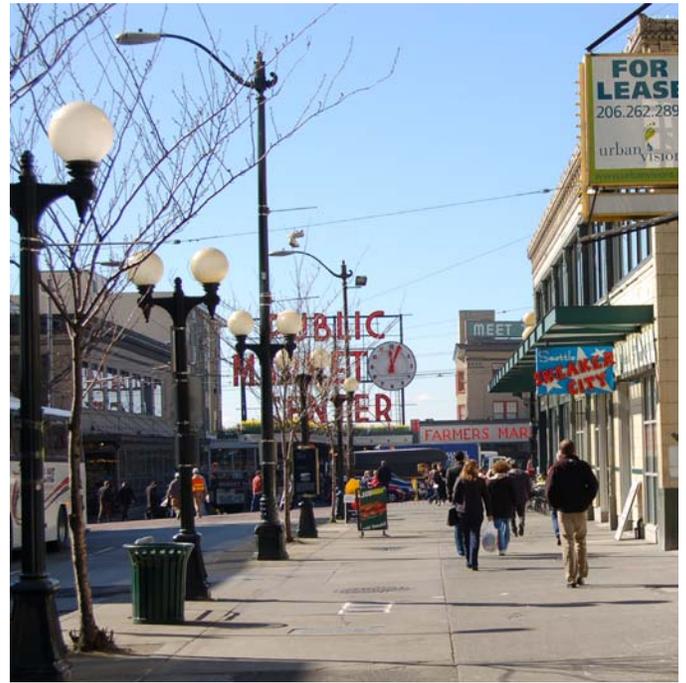
Establish Elements of Continuity and Elements of Differentiation:

Continuity

- 2' x 2' scored concrete sidewalk paving
- Hanging flower baskets
- Surface planting under trees
- Transit and wayfinding signage and markers
- Install string lights in street trees during winter season

Differentiation

- Plantings
- Blade signs (ped-oriented, perpendicular to sidewalk)
- Glass canopies, segmented to express storefronts
- Durable, well designed seating at outdoor cafés, delineated with low, transparent metal railings (optional)
- Unique, pedestrian-oriented elements



Ornamental lighting is an asset on Pike and Pine Streets



Iconographic qualities of Westlake Park (paving, planters, etc.) should remain concentrated

Design Guidelines

The following improvements by applicants to paving, landscape, lighting, and street furniture aim to establish a vibrant Pike & Pine area:

Paving

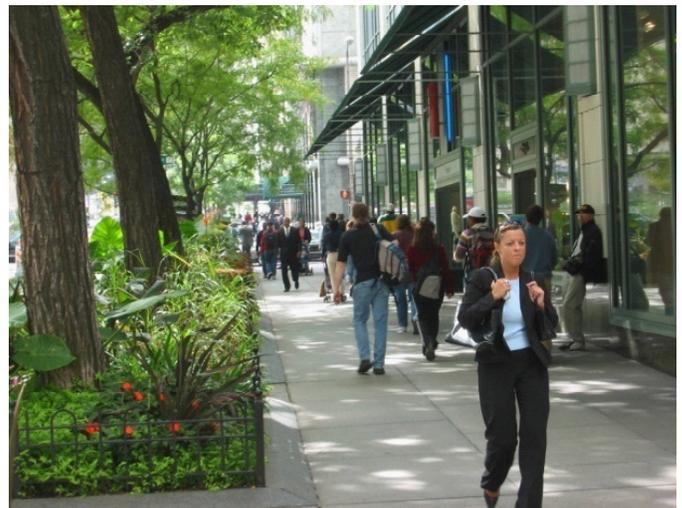
- Sidewalks
 - 2' x 2' scored concrete, medium broom finish
- Crosswalks per SDOT standard
- Bronze inlaid street names at intersections
- Landscape Zone - Paving between planting beds is intended to promote stormwater infiltration and provide visual contrast with surrounding paving. Permeable concrete paving is preferred, as it is naturally darker in color and achieves both of the above objectives. Where permeable paving is not suitable, standard concrete colored with lampblack should be used for visual contrast. Scoring should be consistent with sidewalk zone. Specifications are referenced at the end of the design guidelines section

Landscape

- Provide large, at-grade plant beds to promote tree health; reduce impermeable surface and heat island effect; and provide greater color, texture, and seasonal interest in the Pike/Pine corridor. Provide a safe barrier by clearly defining the edges of planting beds with a curb and fence system:
 - Curb - Charcoal Black granite curbing, 6 inches wide by 10 inches tall, set with top face 4 inches above sidewalk grade. Standard module lengths of 6 feet should be used and cut as necessary to fit planter dimensions, except where curves require radial modules. Granite curbing must be pre-drilled to accommodate fencing and doweling to adjacent sidewalk (See detailed section drawing, p.25)
 - Fence - Constructed of powder-coated steel with gunmetal gray finish, the fence is intended to protect planting beds from pedestrian and pet traffic. It

consists of 1" square vertical elements with 4" spacing, connected by horizontal members, 3/4" x 1", welded to the inside face of vertical members. The finished height of the fence should be 1'-6" above adjacent pavement (see section elevation drawings.)

- Provide plantings that offer variety in texture, color, fragrance, and seasonal interest. Arrange plants informally with taller plant material toward the center and spreading ground covers at the edges. Plantings should be full and offer coverage of the groundplane in planter beds



Surface planting under trees appropriate to the human scale



Seattle's downtown wayfinding kiosks

Design Guidelines

- Design plant layout and/or irrigation to avoid damage to roots of existing trees. Use care when planting, and avoid increasing depth of dirt in planter so as not to stress or harm existing trees
- Include sufficient evergreen “structural” plants in the plant palette to maintain a lush look throughout the year and to serve as a foil for seasonal displays
- Avoid compromising safety and storefront visibility in plant bed design; select plant material that will not obstruct sight lines between the street and storefronts
- Use sustainable strategies for delivery of regular and supplemental water to planting beds, acknowledging heightened requirements during establishment of new plantings and dry summer months thereafter. Where possible, incorporate reuse of stormwater run-off to minimize the use of potable water
- All new street tree planting requires approval by SDOT Urban Forestry (684-TREE). Generally, where honey locust trees are approved for replacement, they will be replaced with a variety of hybrid elm. Other corridor plantings will be approved for continuity with the existing species selected for the corridor
 - Applicants proposing to replace existing trees must contact SDOT Urban Forestry for prior approval. SDOT Urban Forestry will work with SPU and SCL on the applicant’s behalf to verify whether root barriers or tree pits are necessary to protect subsurface water mains or electrical ducts
 - The conceptual design strives to avoid conflicts with utilities, but applicants and the City must field-verify utility locations before installation of new trees. To streamline customer service, SDOT Street Use will serve as applicants’ single point of contact and will coordinate utility review on applicants’ behalf.
 - For clearances, see section 4.20 of the Right of Way Improvements Manual. Note in this plan area the standard clearance required between curbs and the edge of the under-tree planter has been reduced to 1.5 feet, on those blocks with no adjacent parking.
- Install hanging flower baskets on building frontages and on ornamental street light poles where permitted.
- The city intends to work with the Downtown Seattle Association (DSA) and the business community on an implementation and maintenance plan for the seasonal bulb plantings and ground covers included in the planting beds.



Transit signage



Highly transparent transit canopies in Portland, Oregon



Surface plantings on Fifth Avenue

Lighting

- Retain ornamental lighting on Pike and Pine streets while employing energy-saving fixtures and taking measures to reduce light pollution
- Enhance lighting at transit waiting areas by lighting building canopies
- Clearance requirements between new trees and light poles are found in section 4.20 of The Right of Way Improvements Manual

Street Furniture

- Retain existing litter and recycling receptacles provided and maintained by the city. Those which conflict with the location of planned planting beds can be moved to the nearest edge of the planting bed.
- Incorporate the city's downtown wayfinding project signs and kiosks
- Incorporate bike racks parallel to the curb to encourage bicycling by providing safe and convenient bicycle parking
- When a free-standing transit canopy is used, adopt a highly transparent design
- Incorporate Metro transit signage
- Fencing for new sidewalk cafés should carry forward the design and materials for the fences associated with the under-tree planting areas (see page 25)

Utilities/Areaways

- Locate proposed landscaping and street furnishings to avoid utility and areaway conflicts
- Required clearances between trees and other infrastructure may be found in section 4.20 of the Right of Way Improvements Manual



Seattle's famous Pike Street icon

Maintenance

- The success of all streetscape improvements relies heavily on appropriate private sector maintenance plans. Maintenance of the abutting sidewalk area remains the responsibility of the property owner
- Coordinate maintenance efforts to improve efficiencies and ensure positive results
- To help the city reduce storm drain maintenance costs and keep ratepayers' utility rates low, do not wash, sweep, or blow trash or planting materials into gutters

Additional Resources and References

- Suggested strategies for greening infrastructure installations, such as the use of trellises, collection of rainwater, and the use of permeable pavement can be found on the city's Green Factor website (Note: Green Factor is a requirement only for new development in neighborhood commercial zones. However the menu of options may be of interest more broadly): <http://www.seattle.gov/dpd/Permits/GreenFactor/>
- Required clearances between trees and other infrastructure may be found here: www.seattle.gov/transportation/rowmanual/manual/4_20.asp Note that in this project area reduced clearances are permitted between the curb and the under-tree planter in some blocks
- Permeable pavement specifications are included in the Right-of-Way Improvements Manual, Chapter 6, section 4. Applicants and designers should be aware that the color and texture of permeable pavement differs from that of the standard concrete, and designs should take these differences into account
- Free bike rack installation options are described at: <http://www.seattle.gov/transportation/bikeracks.htm>
- The sidewalk café permitting process has been streamlined for greater efficiency. The sidewalk café client assistance (CAM) provides more information, available at: www.seattle.gov/transportation/stuse_docs.htm

Streetscape Conceptual Plans

List of Conceptual Plans:

Prototypical Sections

Plan - Prototypical Treatment at Plant Beds

Pike Street - 100 Block

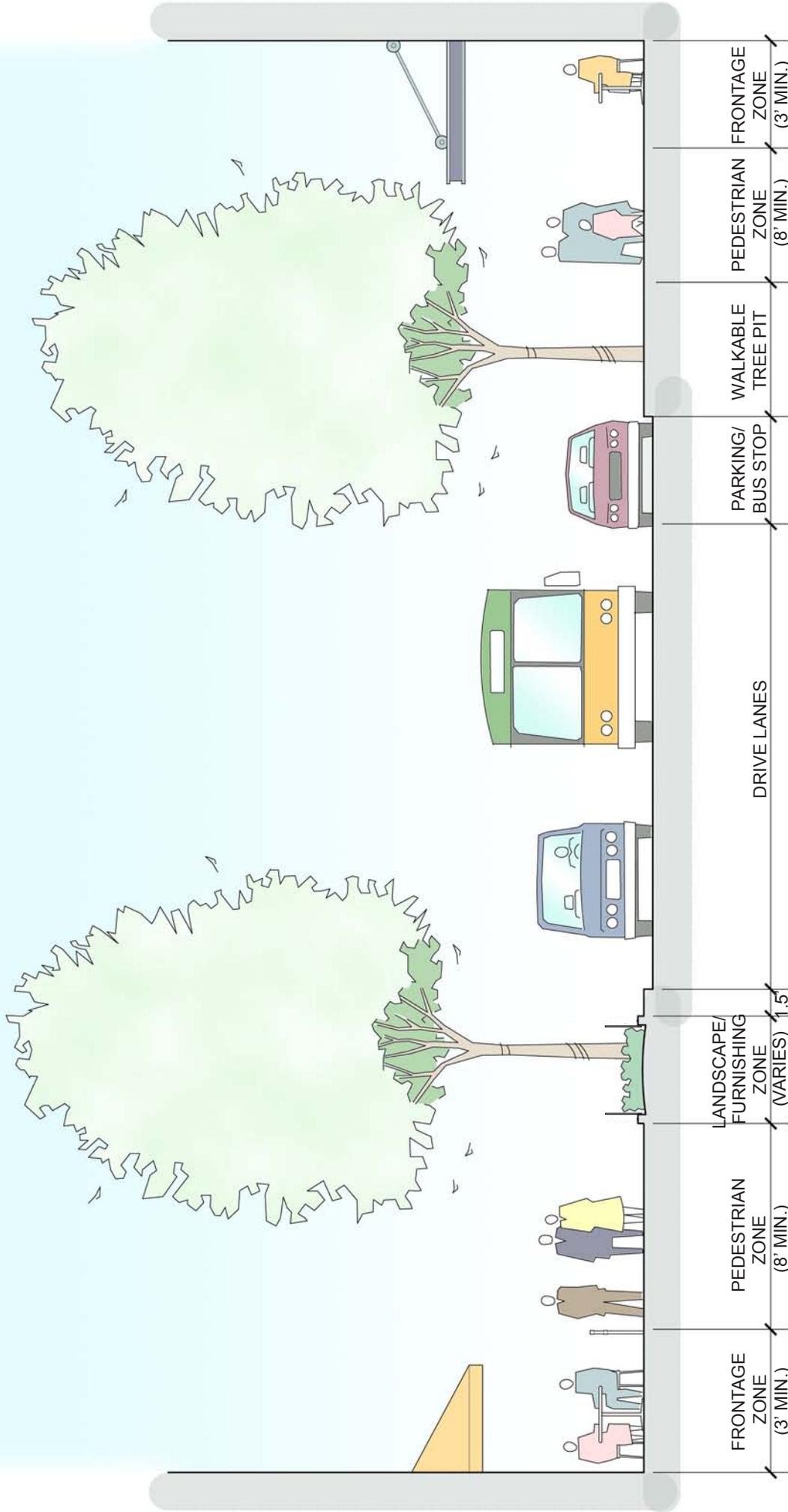
Pike Street - 200 Block

Pike Street - 300 Block

Pine Street - 100 Block

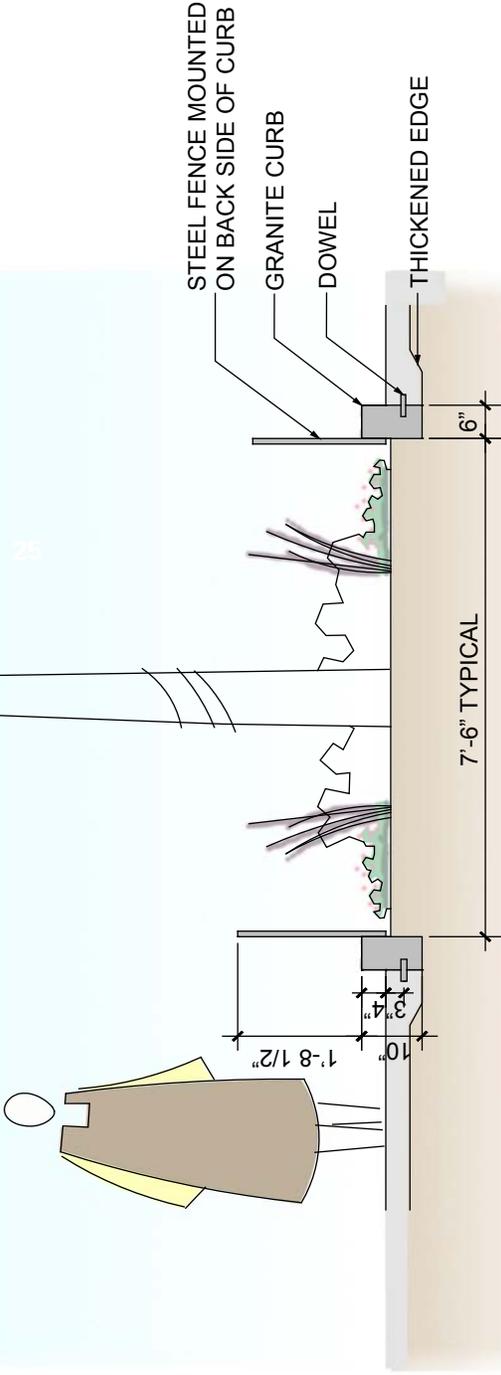
Pine Street - 200 Block

Pine Street - 300 Block

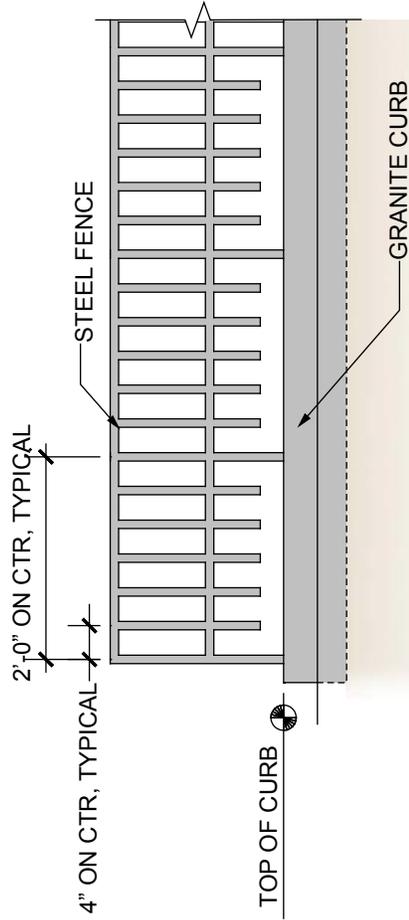


PIKE & PINE STREETSCAPE CONCEPTUAL DESIGN

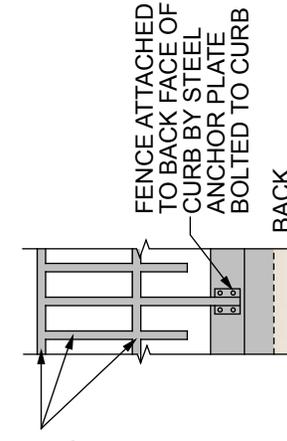
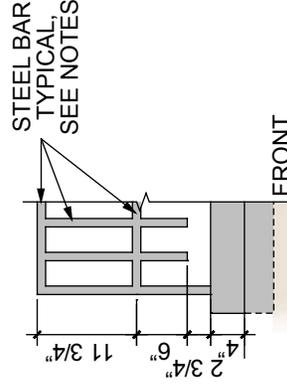
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SECTION THROUGH PLANTER



CURB AND FENCE ELEVATIONS



NOTES:

FENCE BARS 1"x1" STEEL.

FENCE FINISHED WITH RUST-INHIBITING PRIMER AND TOP-COAT OF THERMOSETTING POLYESTER POWDERCOAT RESISTANT TO UV, CHIPPING AND FLAKING.

COLOR SHALL BE GUNMETAL GRAY.

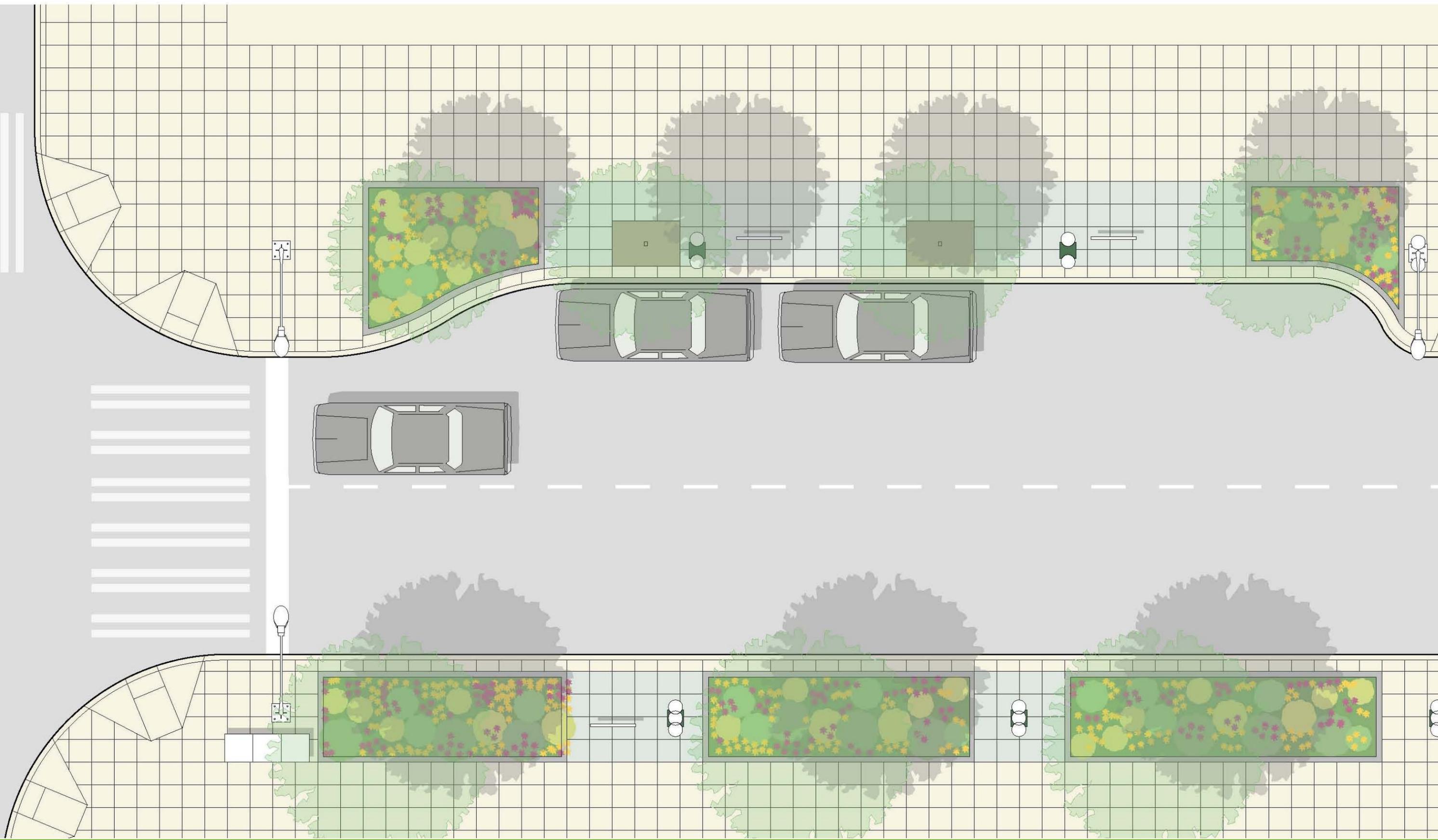
ALL FENCE CONNECTIONS WELDED.

RAISED CURB CHARCOAL BLACK GRANITE. THERMAL FINISH ON FACES AND CORNERS. CURB DOWELED TO SIDEWALK 24" O.C., ALTERNATING WITH FENCEPOSTS.

DESIGN IS CONCEPTUAL AND NOT INTENDED AS CONSTRUCTION DOCUMENT.

DESIGN OF SIDEWALK CAFE FENCING IN THE AREA IS ENCOURAGED TO FOLLOW THE PLANTER FENCE DESIGN PRECEDENT.

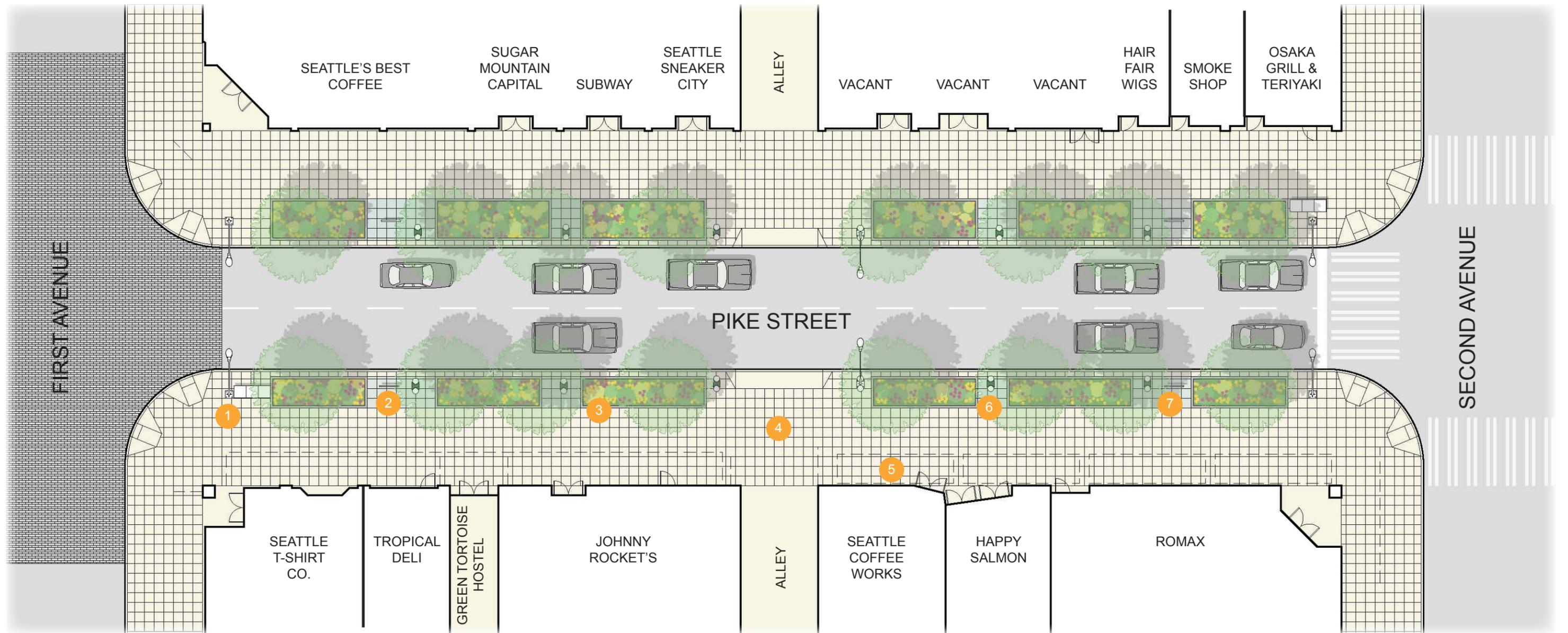




Plan - Prototypical Treatment at Plant Beds



PIKE & PINE STREETScape CONCEPTUAL DESIGN



- 1 CHIEF SEATTLE LIGHTS AT BLOCK ENDS

2 TWO BIKE RACKS MID-BLOCK, PARALLEL TO STREET, COORDINATED WITH ADJACENT USES

3 AT-GRADE LANDSCAPE BEDS WITH RAISED CURB AND FENCE

4 CAST-IN-PLACE CONCRETE PAVEMENT, BROOM FINISH, 2' x 2' SCORING PATTERN
- 5 OVERHEAD CANOPIES

6 6" WIDE PAVED PADS AT LIGHT FIXTURES IN LANDSCAPE BEDS

7 COLORED CONCRETE PAVEMENT BETWEEN PLANTERS

PIKE STREET - 100 BLOCK

PIKE & PINE STREETScape CONCEPTUAL DESIGN





- 1 CHIEF SEATTLE LIGHTS AT BLOCK ENDS
- 2 TWO BIKE RACKS MID-BLOCK, PARALLEL TO STREET, COORDINATED WITH ADJECENT USES
- 3 AT-GRADE LANDSCAPE BEDS WITH RAISED CURB AND FENCE
- 4 CAST-IN-PLACE CONCRETE PAVEMENT, BROOM FINISH, 2' x 2' SCORING PATTERN

- 5 OVERHEAD CANOPIES
- 6 6" WIDE PAVED PADS AT LIGHT FIXTURES IN LANDSCAPE BEDS
- 7 NEW TREES - SEE DESIGN GUIDELINES, P.20
- 8 COLORED CONCRETE PAVEMENT BETWEEN PLANTERS

PIKE STREET - 200 BLOCK

PIKE & PINE STREETScape CONCEPTUAL DESIGN



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- 1 CHIEF SEATTLE LIGHTS AT BLOCK ENDS
- 2 ONE BIKE RACK MID-BLOCK, PARALLEL TO STREET, COORDINATED WITH ADJACENT USES
- 3 AT-GRADE LANDSCAPE BEDS WITH RAISED CURB AND FENCE
- 4 CAST-IN-PLACE CONCRETE PAVEMENT, BROOM FINISH, 2' x 2' SCORING PATTERN
- 5 OVERHEAD CANOPIES

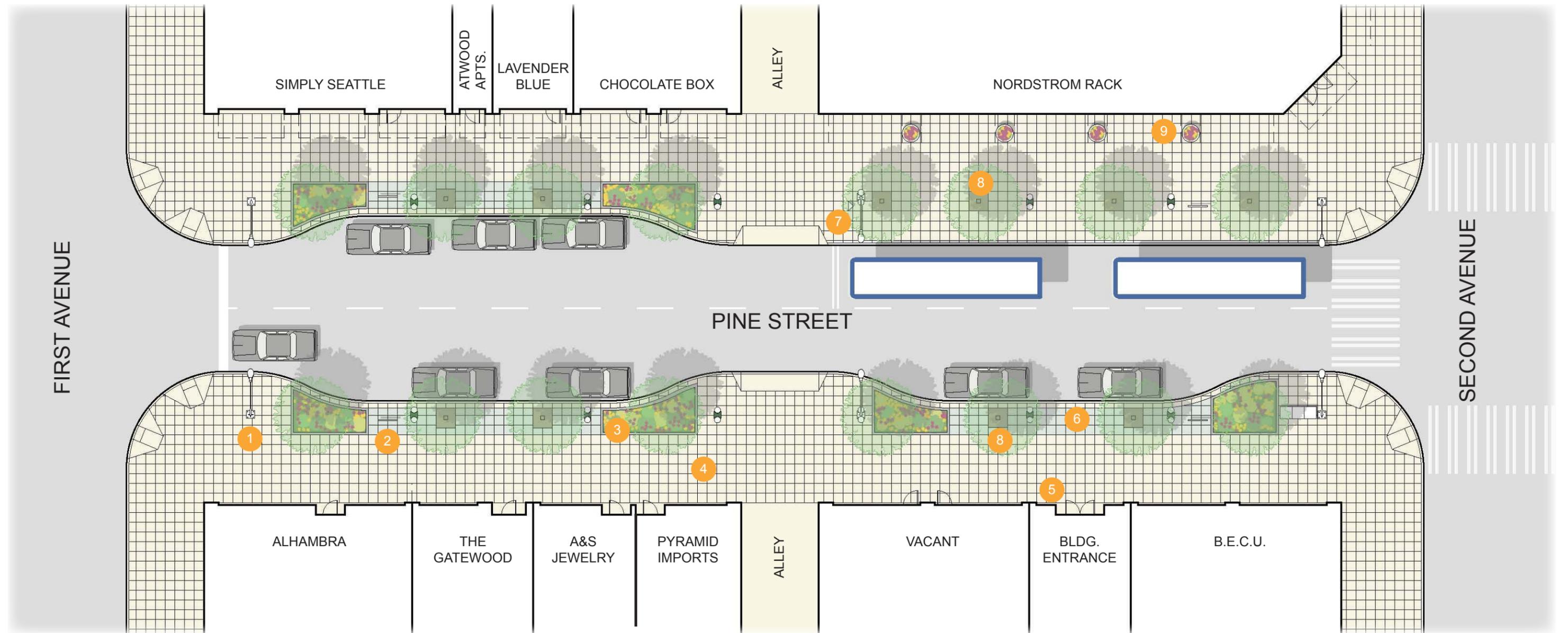
- 6 6" WIDE PAVED PADS AT LIGHT FIXTURES IN LANDSCAPE BEDS
- 7 TRANSIT SIGNAGE
- 8 REMOVE BUS SHELTERS, REPLACE WITH BENCHES AND RAISED PLANTERS AGAINST BUILDING
- 9 COLORED CONCRETE PAVEMENT BETWEEN PLANTERS

PIKE STREET - 300 BLOCK

PIKE & PINE STREETScape CONCEPTUAL DESIGN



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- 1 CHIEF SEATTLE LIGHTS AT BLOCK ENDS
- 2 TWO BIKE RACKS MID-BLOCK, PARALLEL TO STREET, COORDINATED WITH ADJACENT USES
- 3 AT-GRADE LANDSCAPE BEDS WITH RAISED CURB AND FENCE
- 4 CAST-IN-PLACE CONCRETE PAVEMENT, BROOM FINISH, 2' x 2' SCORING PATTERN
- 5 OVERHEAD CANOPIES

- 6 COLORED CONCRETE PAVEMENT BETWEEN PLANTERS
- 7 TRANSIT SIGNAGE
- 8 WALKABLE TREE PIT ADJACENT TO PARALLEL PARKING AND AT BUS STOP
- 9 RAISED PLANTERS AGAINST BUILDING

PINE STREET - 100 BLOCK

PIKE & PINE STREETScape CONCEPTUAL DESIGN



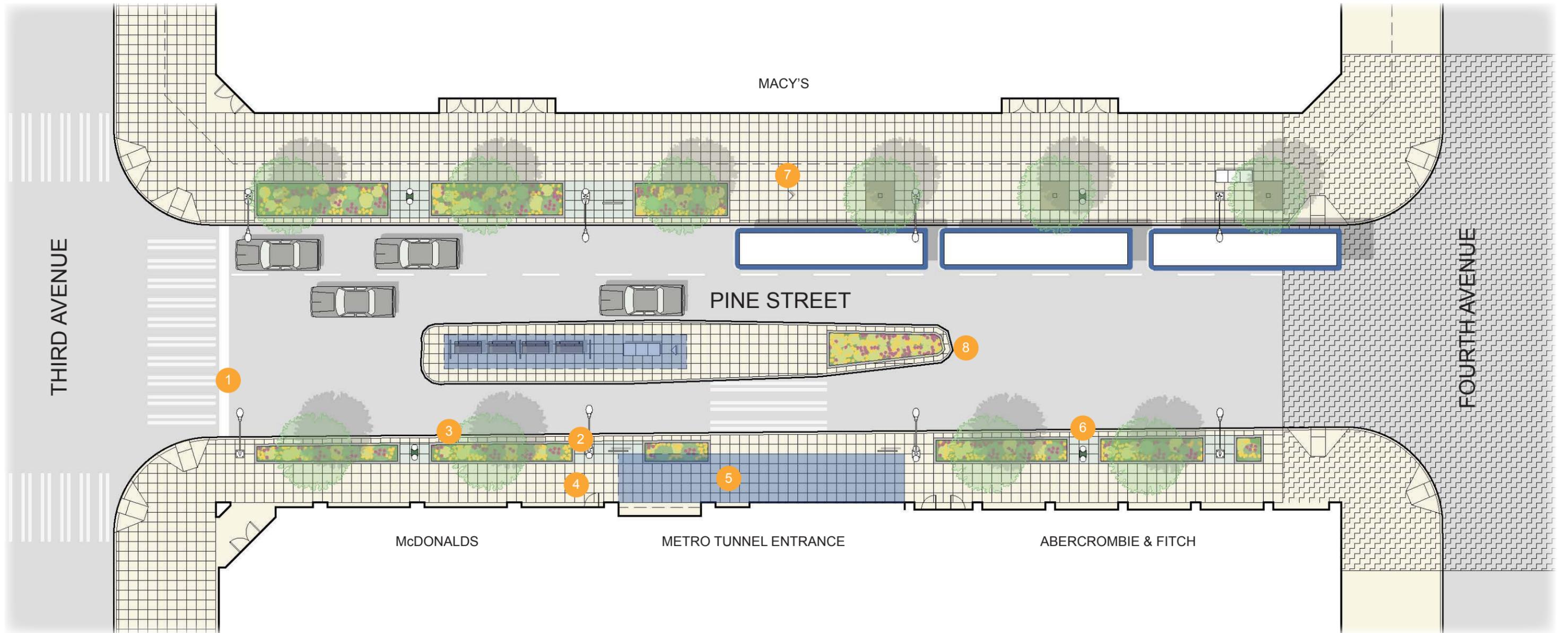


- 1 CHIEF SEATTLE LIGHTS AT BLOCK ENDS
- 2 TWO BIKE RACKS MID-BLOCK, PARALLEL TO STREET, COORDINATED WITH ADJACENT USES
- 3 AT-GRADE LANDSCAPE BEDS WITH RAISED CURB AND FENCE
- 4 CAST-IN-PLACE CONCRETE PAVEMENT, BROOM FINISH, 2' x 2' SCORING PATTERN
- 5 OVERHEAD CANOPIES
- 6 COLORED CONCRETE PAVING BETWEEN PLANTERS
- 8 WALKABLE TREE PIT ADJACENT TO PARALLEL PARKING
- 9 NEW TREE - SEE DESIGN GUIDELINES, P.20

PINE STREET - 200 BLOCK

PIKE & PINE STREETScape CONCEPTUAL DESIGN





- 1 CHIEF SEATTLE LIGHTS AT BLOCK ENDS
- 2 TWO BIKE RACKS MID-BLOCK, PARALLEL TO STREET, COORDINATED WITH ADJACENT USES
- 3 AT-GRADE LANDSCAPE BEDS WITH RAISED CURB AND FENCE
- 4 CAST-IN-PLACE CONCRETE PAVEMENT, BROOM FINISH, 2' x 2' SCORING PATTERN - NOTE CITY VARIANCE AT THIS LOCATION FROM 8' TO 6' MIN. WIDTH TO ALLOW FOR SURFACE PLANTINGS

- 5 OVERHEAD CANOPIES
- 6 COLORED CONCRETE PAVEMENT BETWEEN PLANTERS
- 7 TRANSIT SIGNAGE
- 8 AT-GRADE LANDSCAPE BED IN ISLAND - TO BE REMOVED IN LATER PHASE

NOTE: REDUCTION OF MINIMUM SIDEWALK WIDTH TO 6' IS APPROVED FOR THE SOUTH SIDE OF 300 BLOCK OF PINE.

PINE STREET - 300 BLOCK

PIKE & PINE STREETScape CONCEPTUAL DESIGN

