

3rd Ave S and S Main St Bus Stop Improvements

Pioneer Square Preservation District Certificate of Approval



Core Team



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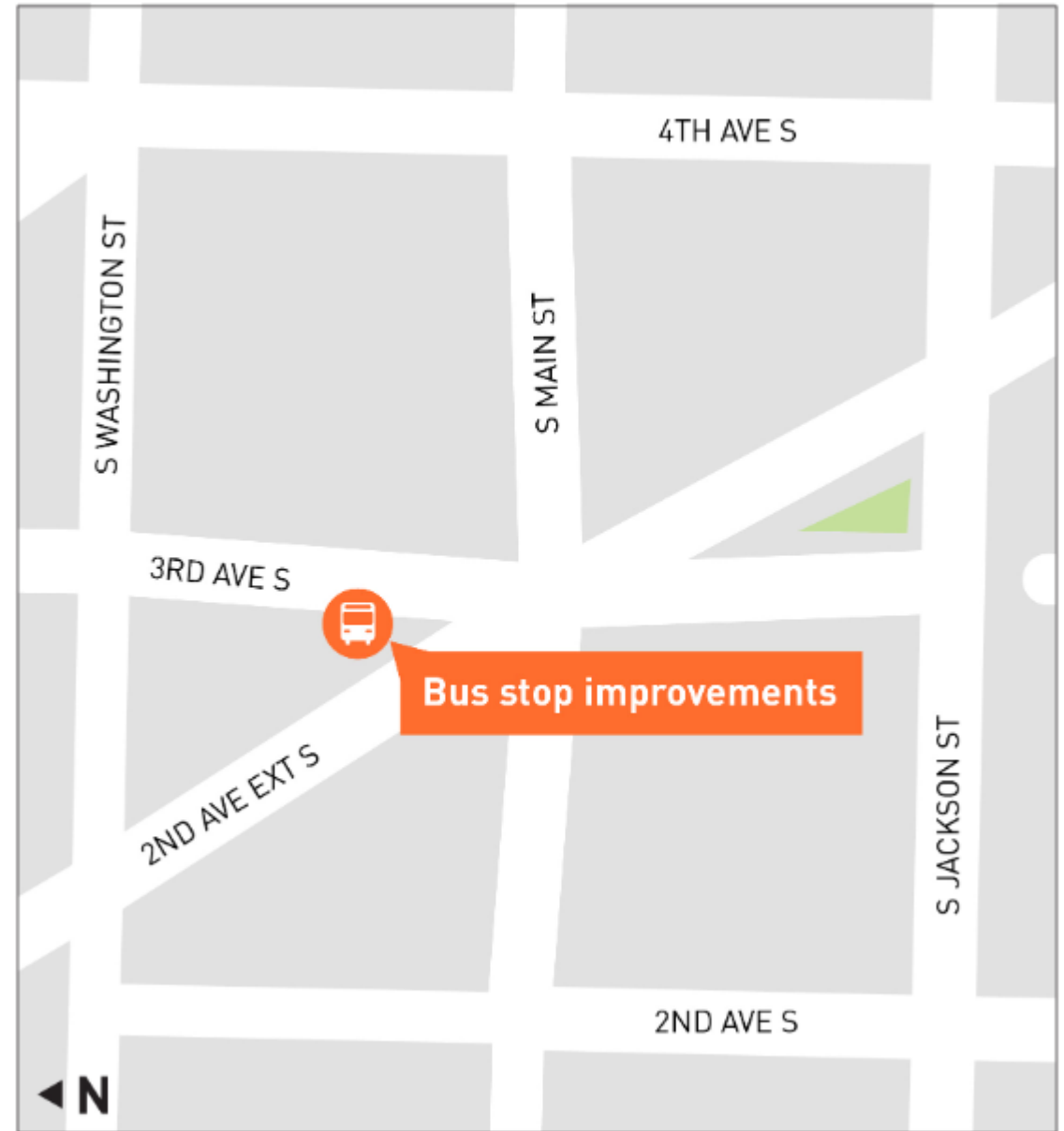
Agenda

- Project overview and background
- Review 100% design plan
- Schedule
- Outreach
- Discussion and feedback

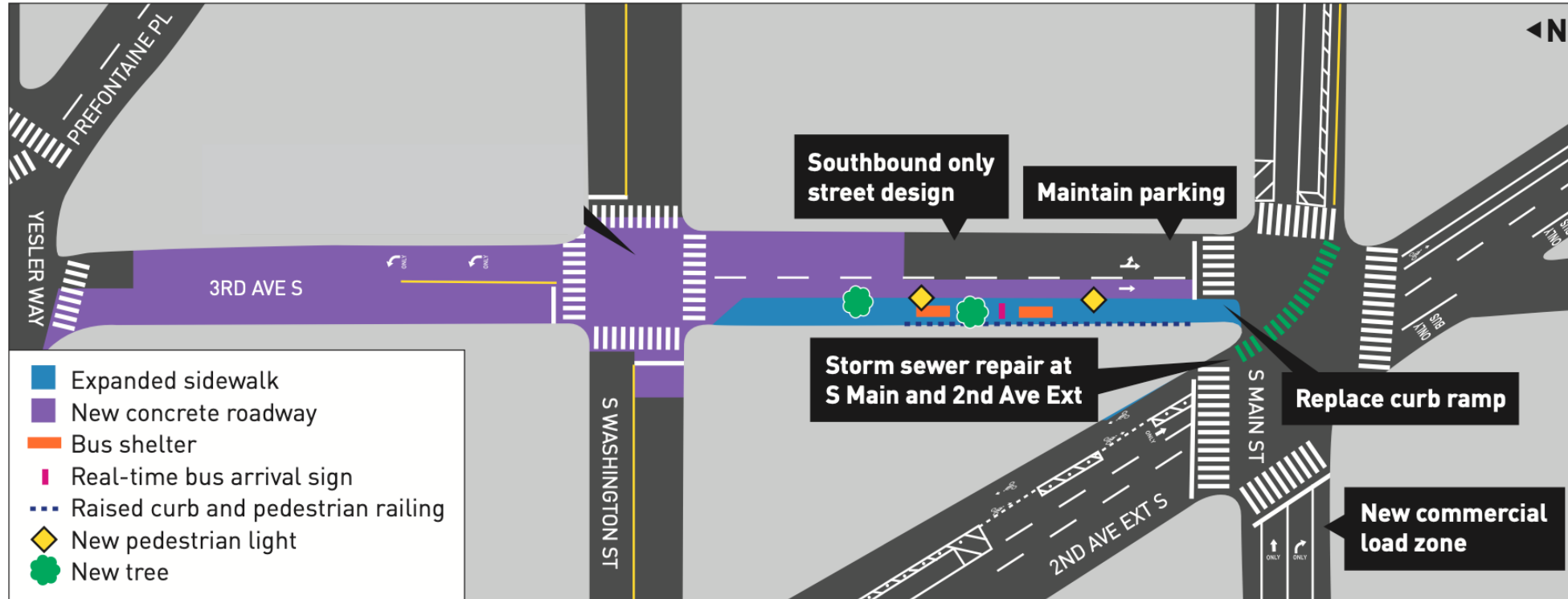


Background

- First identified as part of 3rd Ave Transit Corridor Improvement Project in 2015
- 10th busiest bus stop with 2,600 riders each weekday
- Existing stop lacks accessibility and design features of other stops on 3rd Ave
- Funded with FTA grants and local SDOT, Metro and SPU funding



100% Design Plan



- Widen sidewalks, add pedestrian lighting, improve drainage, plant new trees, re-channelization.
- Bus stop improvements: shelters, seating, real-time arrival info, ORCA card reader
- New concrete pavement between S Yesler St and S Washington St
- Maintain parking and change circulation between Main and Washington to be southbound only
- Not impacting existing areaway, granite curb will remain in place where feasible
- Repair SPU's storm sewer

Pioneer Square Historic Elements

- **Street lighting** - three-globe Chief Seattle bronze base light fixtures.
- **Sidewalk treatment** - one pound lamp-black per cubic yard of concrete, scored at two-foot intervals. There are no prism lights within the project impact area.
- **Granite curbs** - granite curbs will remain in place where feasible.
- **Street furniture**
 - Bus shelter
 - Trash receptacle
 - RTIS sign
- **Street trees** - 2 trees and tree pits.
- **Areaways** - project will avoid areaways and provide language in the project specifications to prohibit staging of materials or equipment on areaways and require the use of hand tools within five feet of the areaway.
- **Handrails** - will be painted "Seattle Railroad Green" color.
- **Detectable warning plate** - cast iron

Existing Conditions



3rd Ave S/Main looking north

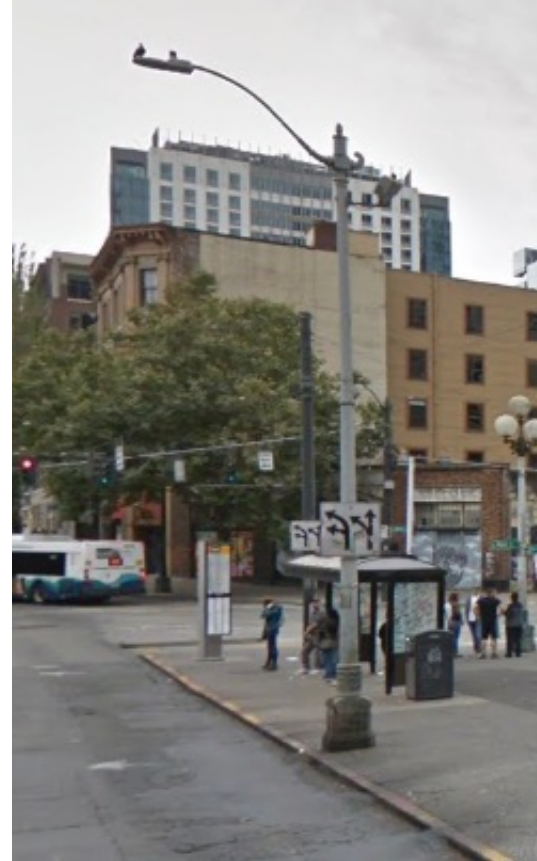


3rd Ave S/Washington looking south

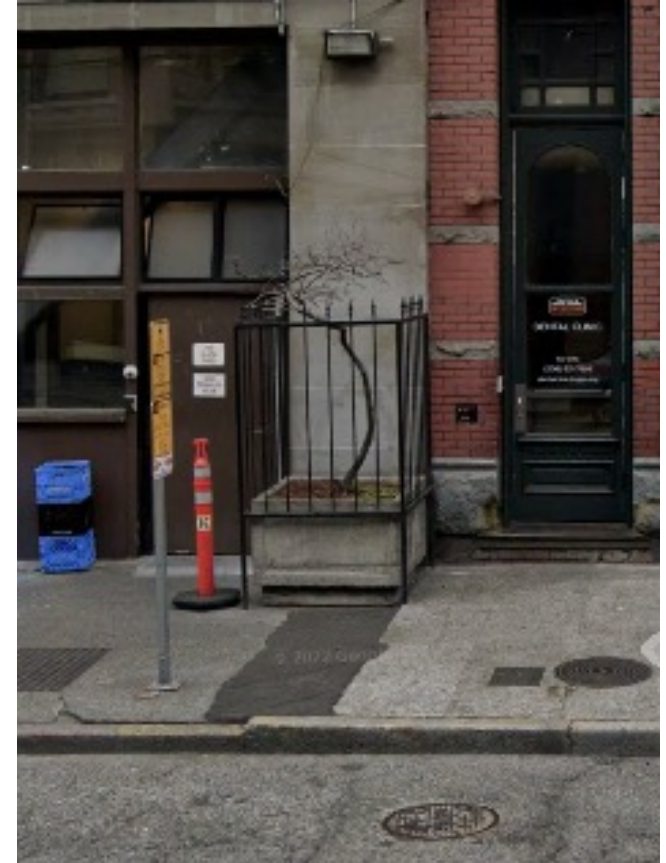
Existing Conditions - Removals



Existing bus shelter and amenities will be removed



Light pole removed/relocated



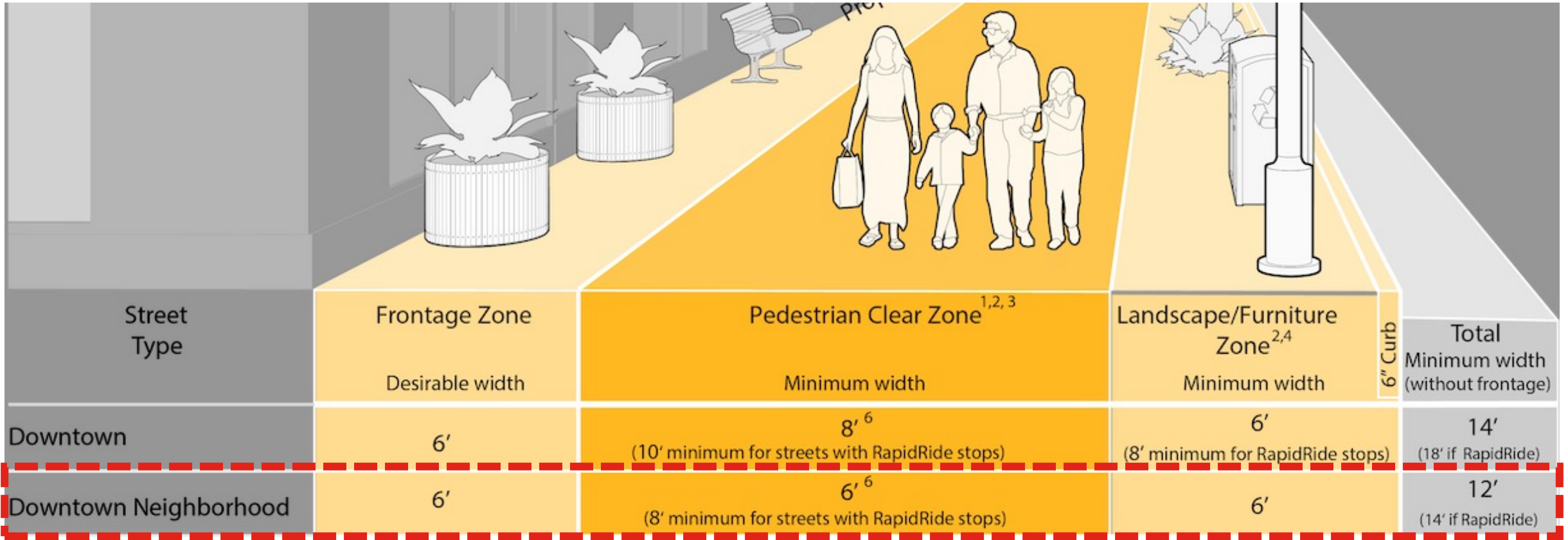
Planter removal

After Construction



Conceptual rendering - see plans for final design configuration

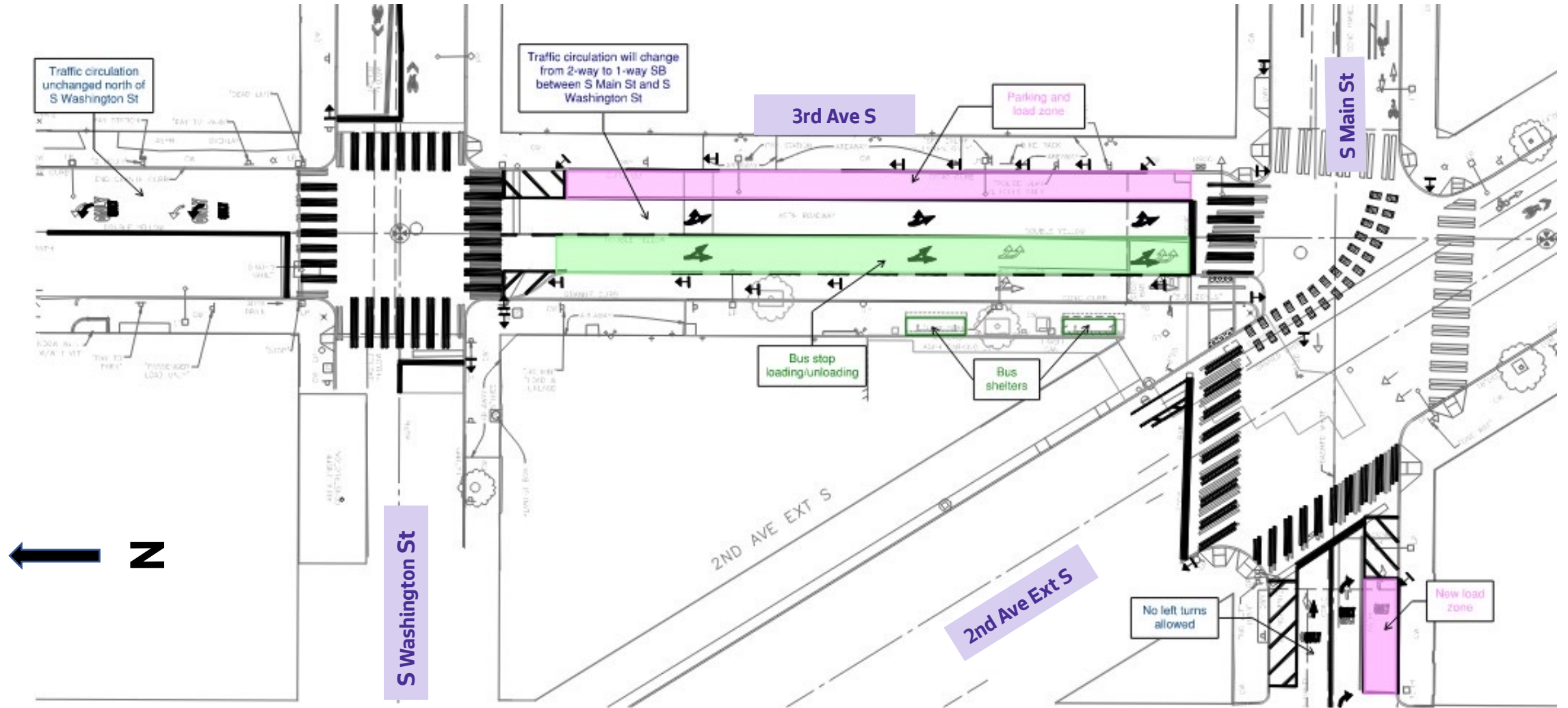
Sidewalk Width Requirements for Downtown Neighborhood Street Type



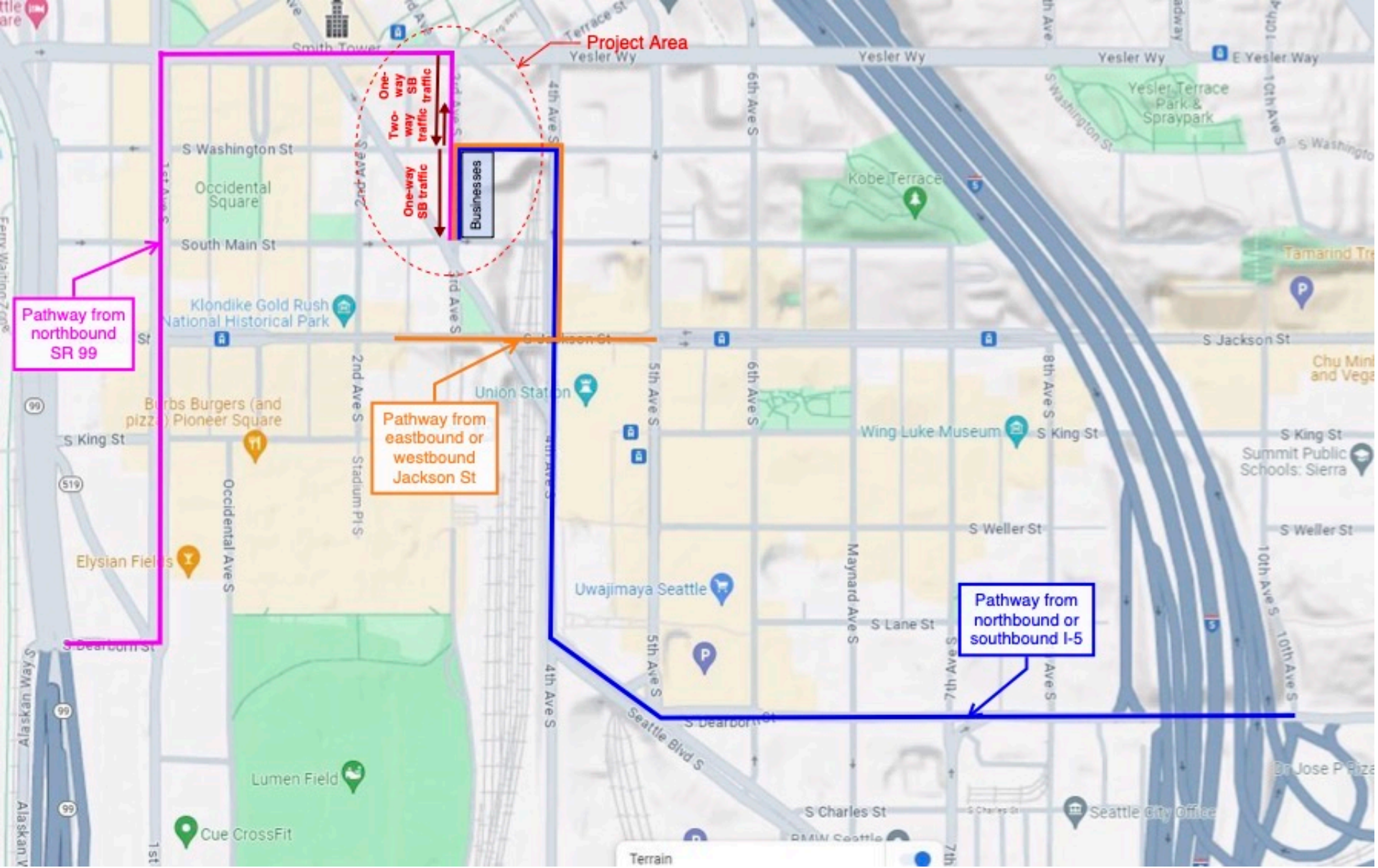
Streets Illustrated Standards - 3rd Ave S is a Downtown Neighborhood street type with future RapidRide service, 20' recommended width.

	Width
Existing sidewalk	9' to 12'
New sidewalk	21'

Channelization Changes


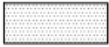










Traffic Circulation to Access New One-way SB Block on 3rd Ave S



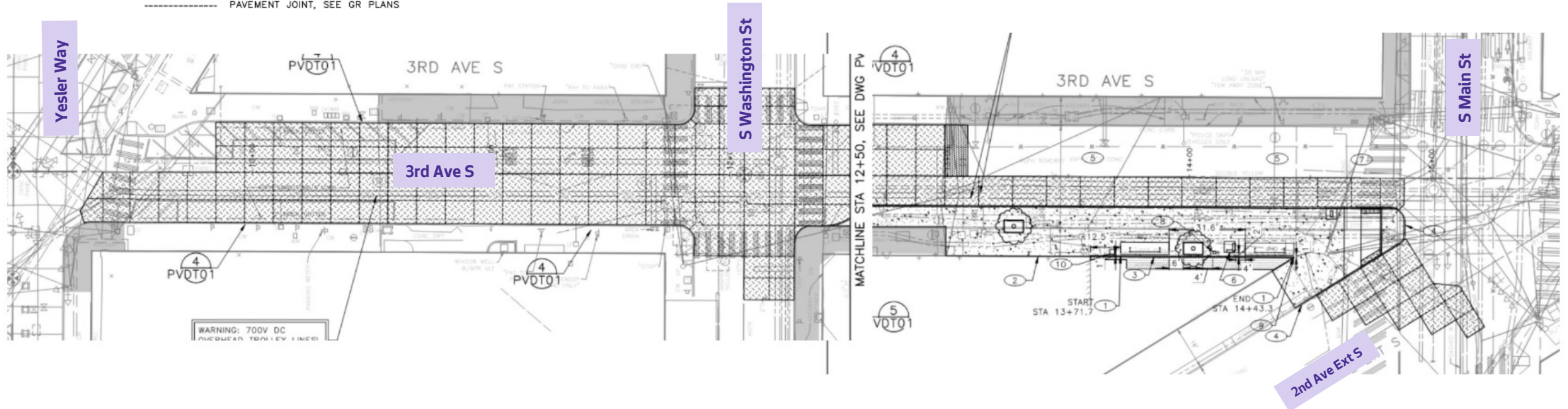
Paving Extents

LEGEND:

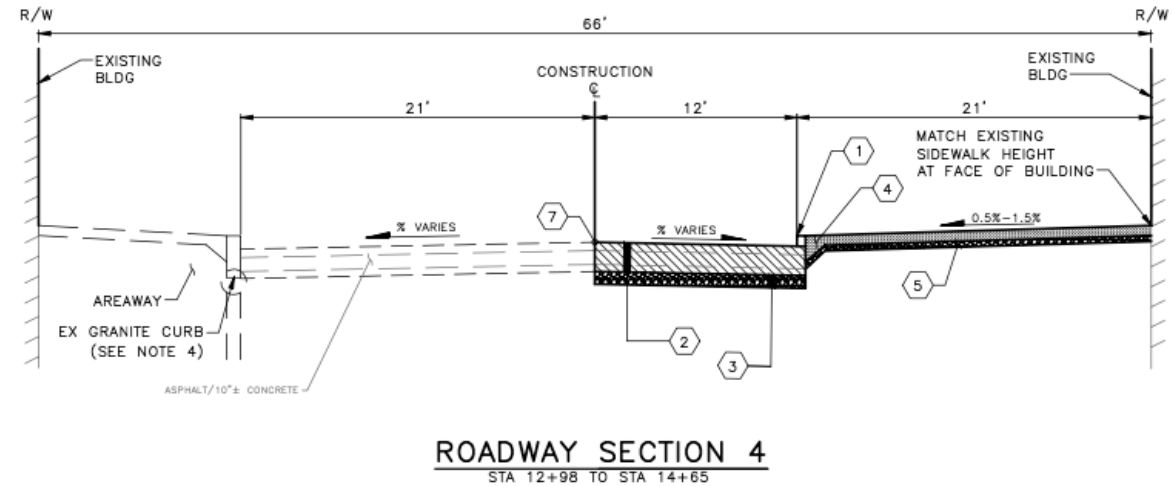
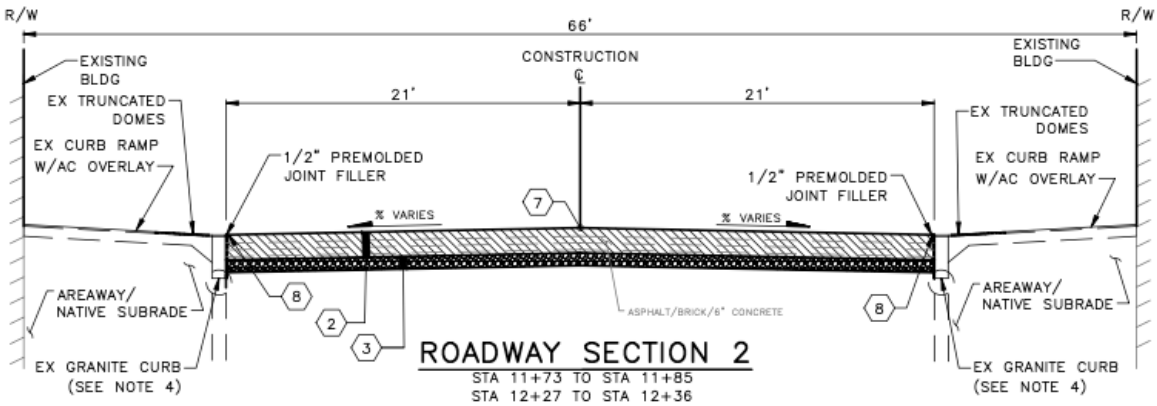
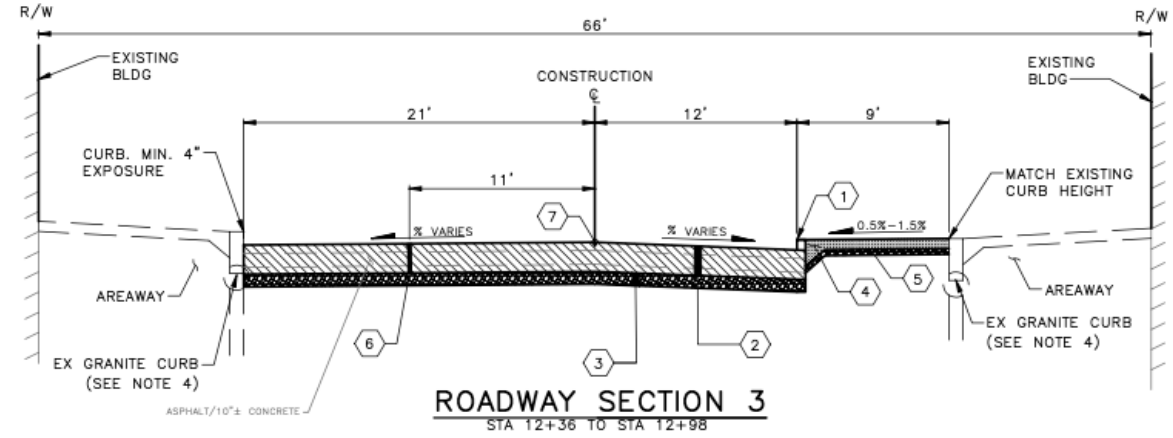
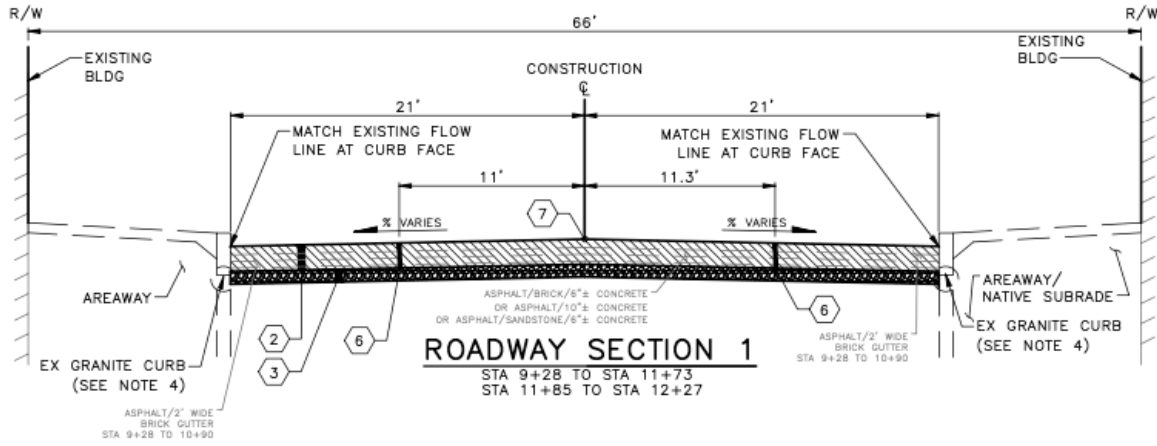
-  14" ROADWAY CEM CONC, HES (24HR), 14 IN OVER MNRL AGG TYPE 2, 6" COMPACTED DEPTH
-  2" HMA CL 1/2" PG 58H-22 OVERLAY
-  COLORED CEM CONC W/ 25% POZZOLAN PER COS STD PLAN 420. REFER TO SECTION 8-14.2
-  EXISTING AREAWAY TO REMAIN, REFER TO SECTION 6-02.1(1)
-  CURB, CEM CONC TYPE 410C
-  5'x8' TREE PIT AND TREE (EMERALD SUNSHINE ELM-ULMUS PROPINQUA JFS-BIEBRICH), 2-2.5"
-  DETECTABLE WARNING PLATE PER COS STD PLAN 422A.
-  PEDESTRIAN RAILING PER STD PLAN 443
-  LIGHT POST, SEE LT PLANS
-  PAVEMENT JOINT, SEE GR PLANS

CONSTRUCTION NOTES:

- 1 INSTALL PEDESTRIAN RAILING PER STD PLAN 443
- 2 SEE DETAIL 1-PVDT1
- 3 INSTALL PEDESTRIAN CURB PER DETAIL 2-PVDT1
- 4 INSTALL CURB PER DETAIL 3-PVDT1
- 5 INSTALL RR-20 SHELTER FOOTING PER KC METRO STD PLAN 1-D101B AND 3-D101B (SHELTER INSTALL BY METRO)
- 6 INSTALL TECH PYLON FOOTING RR-5 PER KC METRO STANDARD PLAN 1-D102 (TECH PYLON INSTALL BY MC METRO)
- 7 INSTALL TYPE 3 SIGN FOUNDATION PER KC METRO STD PLAN 2-D104A (SIGN INSTALL BY KC METRO)
- 8 INSTALL POWER PEDESTAL FOUNDATION PER KC METRO STD PLAN D112
- 9 SEE DETAIL 6-PVDT1
- 10 INSTALL SALVAGED TRASH RECEPTACLE



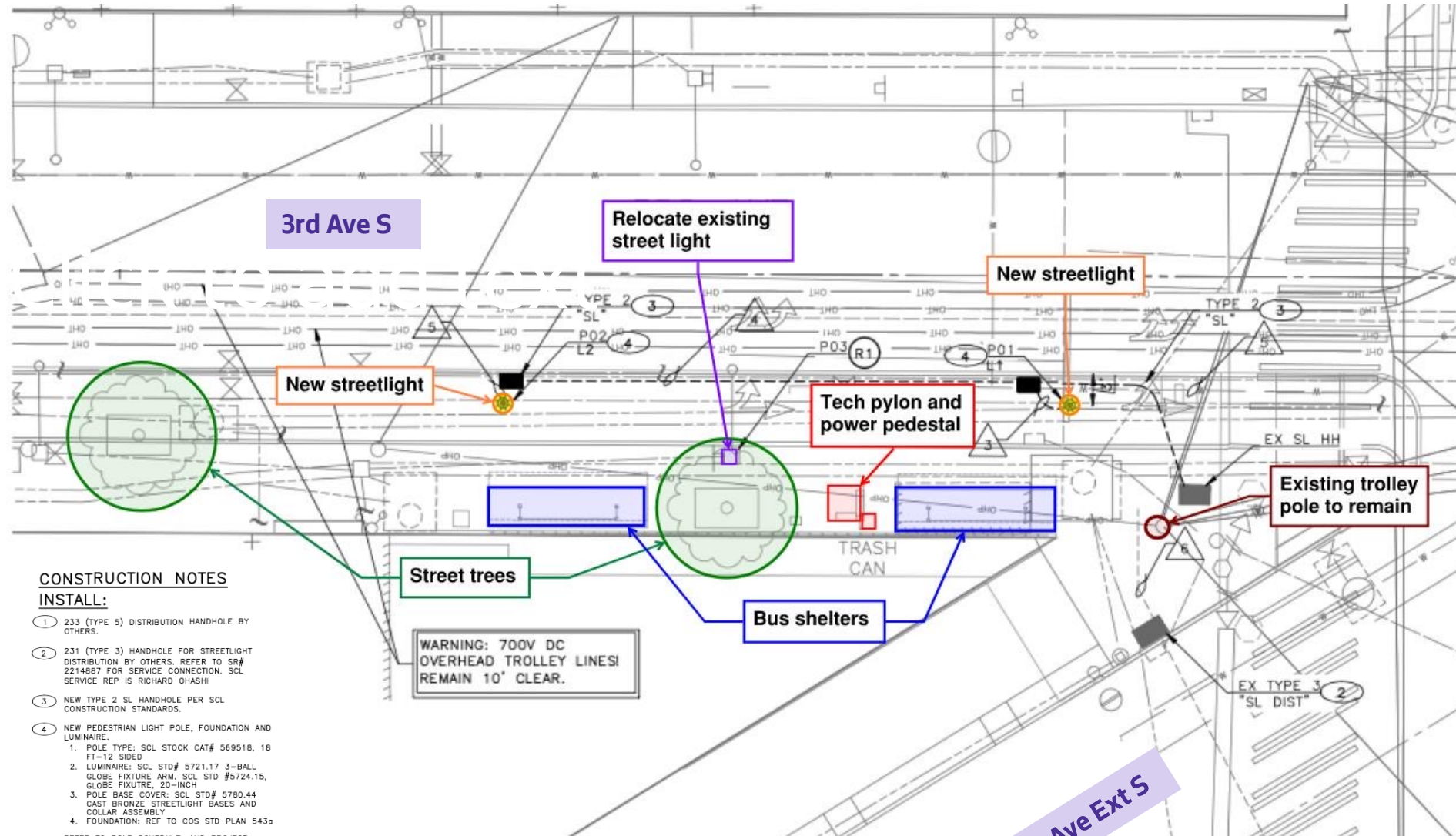
Roadway Sections



LEGEND:

- | | |
|--|---|
| ① CURB, CEM CONC, PER STD PLAN 410C. | ⑤ MNRL AGG TYPE 17 (MINIMUM 6 IN COMPACTED DEPTH) |
| ② ROADWAY CEM CONC (HES 24HR)14 IN | ⑥ LONGITUDNAL PAVEMENT JOINT LOCATION |
| ③ MNRL AGG TYPE 2, MINIMUM 6 IN COMPACTED DEPTH | ⑦ PROFILE GRADE AND PIVOT POINT |
| ④ THICKENED EDGE SIDEWALK (SEE COS STD PLAN 420) | ⑧ SAWCUT 8" FROM EDGE OF EXISTING TRUNCATED DOMES |

Lighting Plan



CONSTRUCTION NOTES

INSTALL:

- ① 233 (TYPE 5) DISTRIBUTION HANDHOLE BY OTHERS.
- ② 231 (TYPE 3) HANDHOLE FOR STREETLIGHT DISTRIBUTION BY OTHERS. REFER TO SR# 2214887 FOR SERVICE CONNECTION. SCL SERVICE REP IS RICHARD OHASHI
- ③ NEW TYPE 2 SL HANDHOLE PER SCL CONSTRUCTION STANDARDS.
- ④ NEW PEDESTRIAN LIGHT POLE, FOUNDATION AND LUMINAIRE.
 1. POLE TYPE: SCL STOCK CAT# 569518, 18 FT-12 SIDED
 2. LUMINAIRE: SCL STD# 5721.17 3-BALL GLOBE FIXTURE ARM, SCL STD #5724.15, GLOBE FIXTURE, 20-INCH
 3. POLE BASE COVER: SCL STD# 5780.44 CAST BRONZE STREETLIGHT BASES AND COLLAR ASSEMBLY
 4. FOUNDATION: REF TO COS STD PLAN 543a
 REFER TO POLE SCHEDULE, AND PROJECT APPENDIX FOR MORE INFORMATION.

REMOVE:

- Ⓡ1 EXISTING STREETLIGHT POLE AND FOUNDATION. RELOCATE CHIEF SEATTLE BASE AND COLLAR TO NEW STREETLIGHT POLE. COORDINATE WITH SCL FOR DEACTIVATION PRIOR TO REMOVAL. REMOVAL MUST NOT OCCUR UNTIL NEW LIGHTS ARE INSTALLED AND COMMISSIONED BY SCL. COORDINATE WITH SCL STREETLIGHT REVIEWER THRU THE ENGINEER TO SCHEDULE SCL CREWS FOR COMMISSIONING. LIGHTS MUST BE SIGNED OFF BY SCL INSPECTOR PRIOR TO REQUESTING SCL CREWS FOR COMMISSIONING.

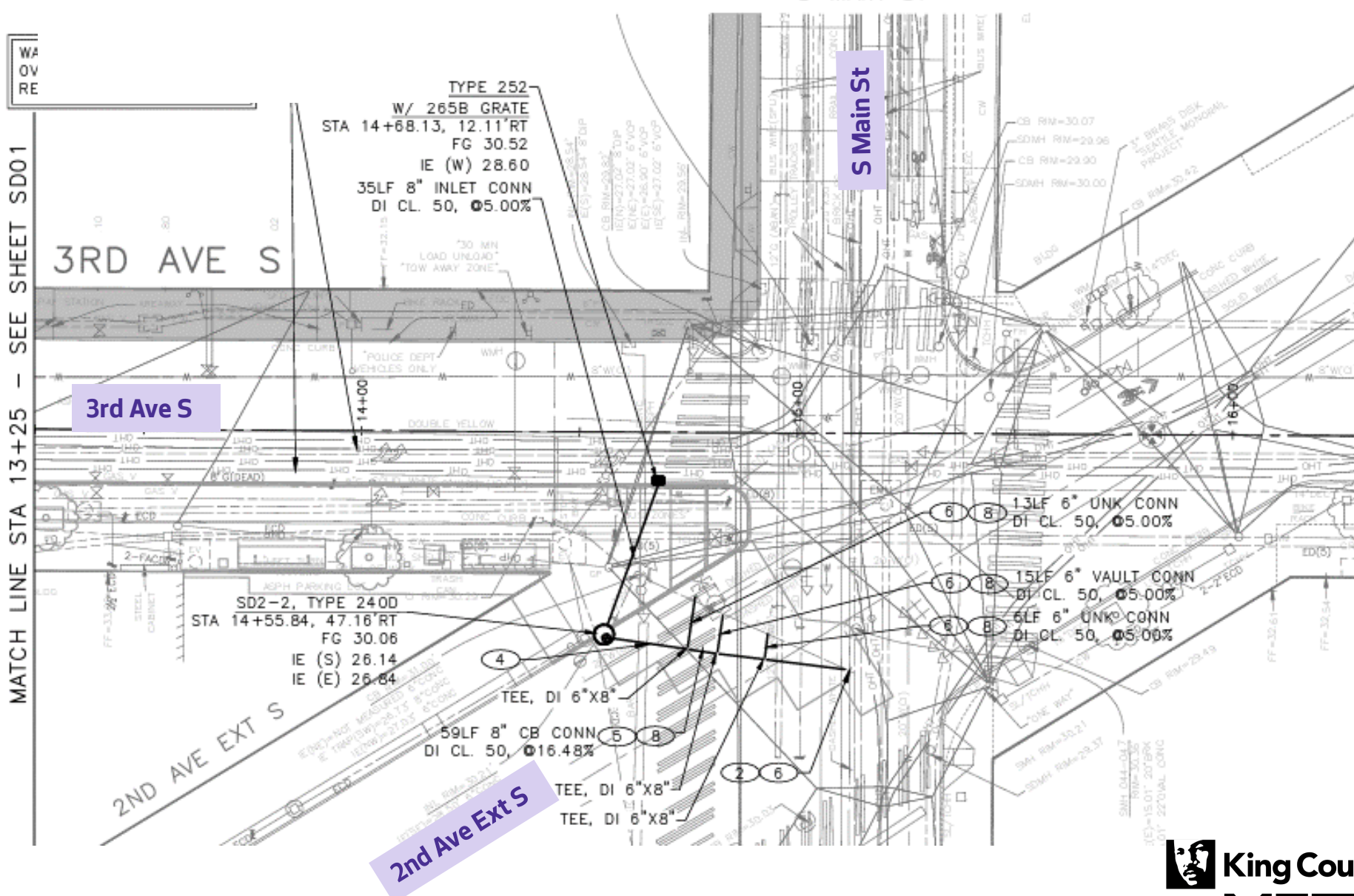
S Main St



Street lighting example

2nd Ave Ext S

Storm Drain Repair for SPU Sewer Spots Program



GENERAL NOTES:

1. INLET AND CATCH BASIN STATIONS AND OFFSETS ARE MEASURED TO THE CENTER OF GRATE AT THE FACE OF CURB (FG) PER STD PLAN 260A AND 260B UNLESS OTHERWISE NOTED ON THE DRAWINGS.
2. RIM ELEV. 1" BELOW FG ELEV. PER STD PLAN 260, UNLESS OTHERWISE NOTED IN THE DRAWINGS.
3. TAPPED CONNECTIONS TO MAINS SHALL BE PERFORMED BY SEATTLE PUBLIC UTILITIES (SPU). TO SCHEDULE CORE TAPS, CONTACT SPU AT (206)615-0511 A MINIMUM OF 48 HOURS IN ADVANCE.
4. IF ROOTS ARE ENCOUNTERED, REFER TO SECTION 8-02.3(7)A FOR PRUNING PROCEDURE.
5. INSTALL POLYETHYLENE FOAM PROTECTION IF LESS THAN 6" CLEARANCE BETWEEN UTILITIES.
6. ALL UTILITIES MUST BE LOCATED AND MARKED PRIOR TO EXCAVATION. REFER TO SECTION 1-07.17.
7. REFER TO SHEET NT01 FOR ADDITIONAL DRAINAGE NOTES.
8. REFER TO APPENDIX FOR GEOTECHNICAL INFORMATION. SURCHARGE LOADING FOR THE STRAIN POLE MUST BE ACCOUNTED FOR IN THE DESIGN OF THE SAFETY SYSTEM.

CONSTRUCTION NOTES:

1. CONNECT NEW PIPE TO EXISTING STRUCTURE. CORE DRILL TO ACCOMMODATE 8" DI PIPE PER SECTION 7-05.3(2) B
2. USE FLEXIBLE SHIELDED COUPLING TO CONNECT TO EXISTING PIPE
3. CORETAP TO BE PERFORMED BY SPU
4. UTILITY SUPPORT PLAN REQUIRED, REFER TO SECTION 2-07.3(4)
5. SUPPORT AND SAFETY SYSTEM REQUIRED, REFER TO SECTION 2-07.3(3) AND 1-07.17(2)A. SEE NOTE 8
6. UTILITY VERIFICATION REQUIRED. REFER TO SECTION 2-04.3(9) AND 1-08.3
7. INSTALL PER DETAIL 1-SD0T01
8. INSTALL PER DETAIL 2-SD0T01
9. PROVIDE CONTRACTOR SUPPORT FOR SPU, REFER TO SECTION 7-15.1

Bus Stop Elements

- Two large bus shelters w/ seating and lean rail
 - "Legacy RapidRide" shelters w/ all-gray paint scheme
 - No RapidRide branding planned at implementation
- Tech pylon w/ real time arrival info & off-board ORCA fare payment
 - "Legacy RapidRide" pylon w/ all-gray paint scheme
 - New ConnectPoint "e-Paper" real-time information screens
- New large route sign & litter receptacle
- Pedestrian railing along back of sidewalk



Route Sign



Litter Receptacle



Tech Pylon



New RTIS Screens



Bus Shelter

Art Opportunity

SDOT & KCM working with 4Culture & artist team to deliver an exterior, site-responsive sculpture for the 3rd & Main bus stop

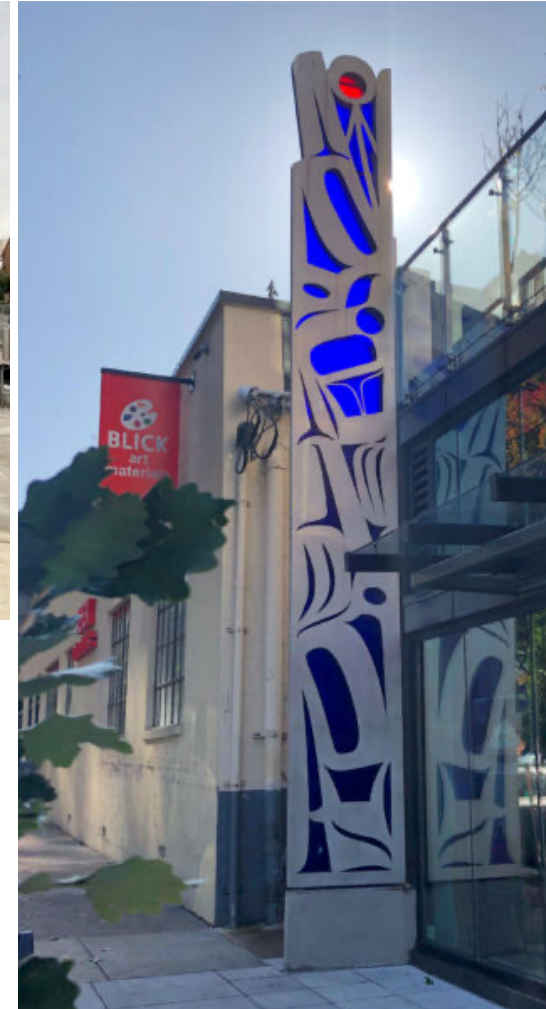
Artists [Preston Singletary](#) & [David Franklin](#) selected in December 2023 and design is expected to kick off soon

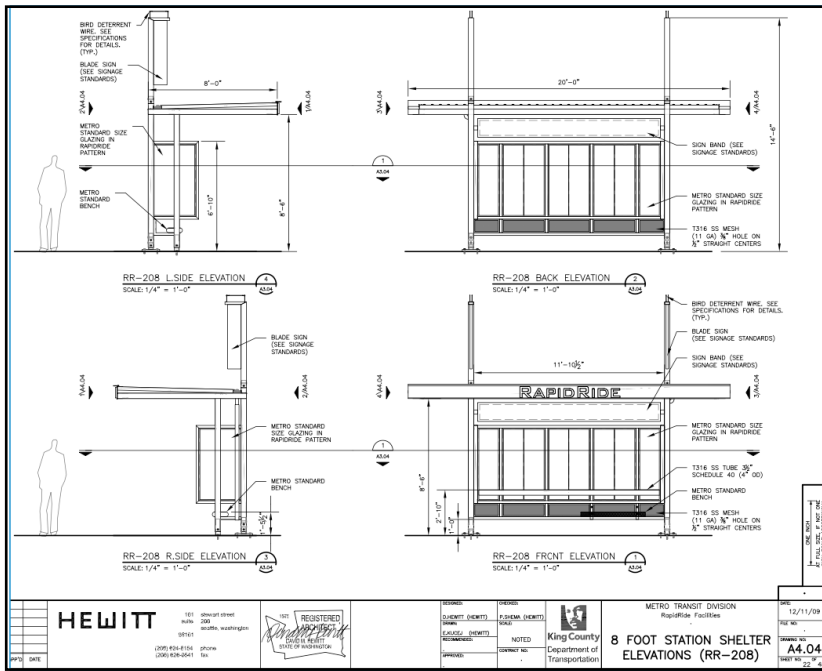
Timeline:

- Design: January-June 2024
- 4Culture PAAC Design Approval: June 2024
- Fabrication: July-April 2025
- Installation: April 2025

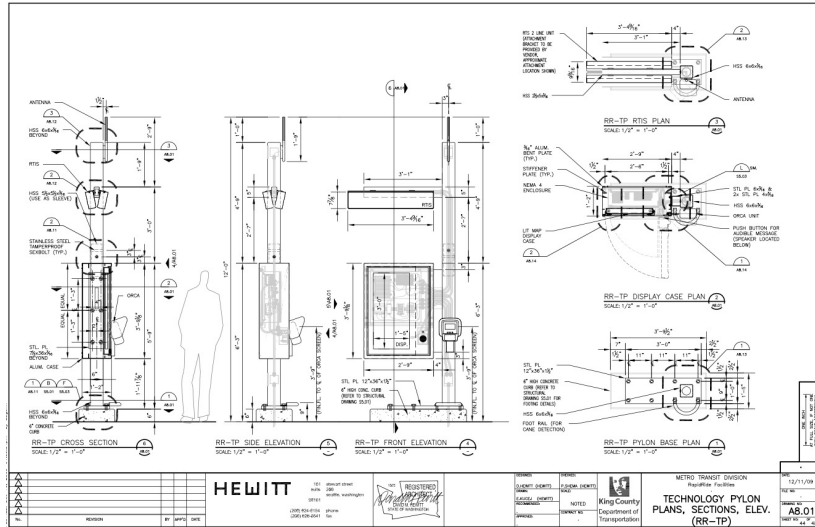


Above: Preston Singletary & David Franklin pose with recent project, **La Diab Pish**, a sculpture depicting a Pacific Giant Octopus installed near Seattle's Climate Pledge Arena; Right: Singletary's and Franklin's 20+ feet tall **Dancing Staff** installed in Portland's Pearl District, represents a Tlingit Dancing Staff. Both sculptures are constructed of steel, glass, and lighting elements.



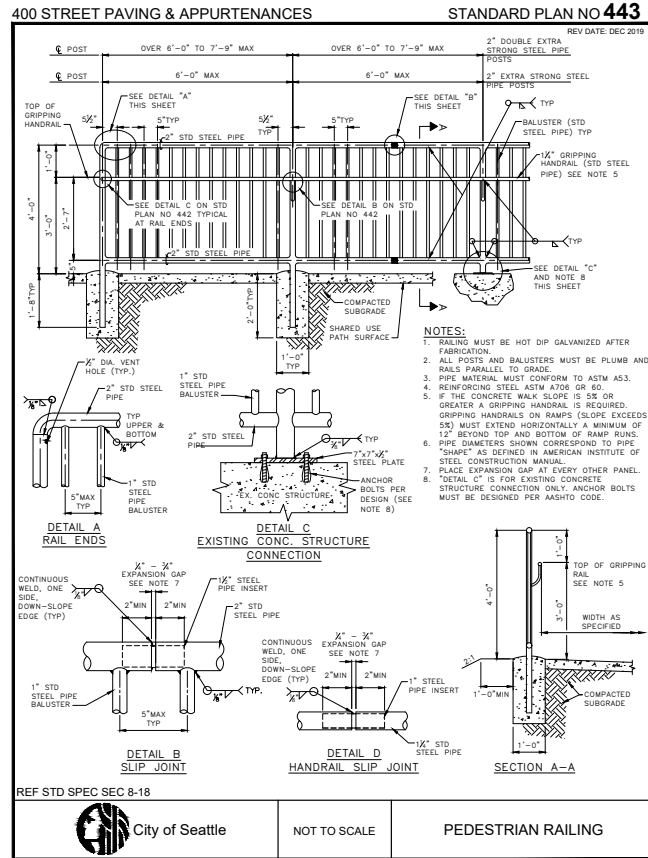


Bus Shelters

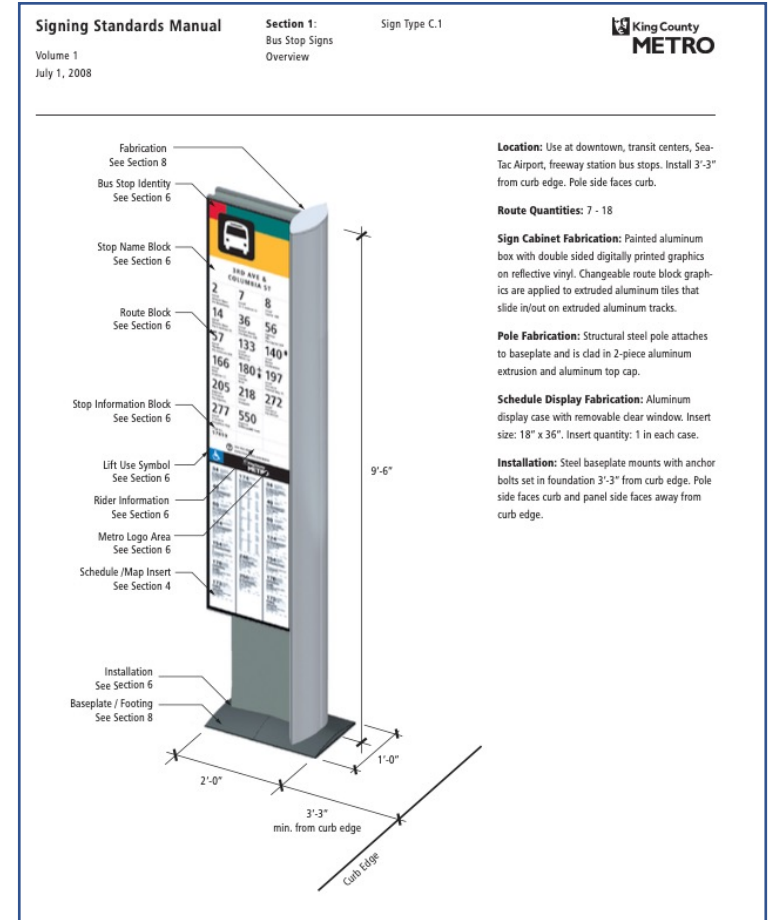


Technology Pylons

Cut Sheets



Handrail



Bus Stop Sign

Location: Use at downtown, transit centers, Sea-Tac Airport, freeway station bus stops. Install 3'-3" from curb edge. Pole side faces curb.

Route Quantities: 7 - 18

Sign Cabinet Fabrication: Painted aluminum box with double sided digitally printed graphics on reflective vinyl. Changeable route block graphics are applied to extruded aluminum tracks that slide in/out on extruded aluminum tracks.

Pole Fabrication: Structural steel pole attaches to baseplate and is clad in 2-piece aluminum extrusion and aluminum top cap.

Schedule Display Fabrication: Aluminum display case with removable clear window. Insert size: 18" x 36". Insert quantity: 1 in each case.

Installation: Steel baseplate mounts with anchor bolts set in foundation 3'-3" from curb edge. Pole side faces curb and panel side faces away from curb edge.

Schedule

- Advertisement: March 2024
- Construction: June 2024 – December 2024



Outreach

- Reached out to nearby businesses
- Briefings with Alliance for Pioneer Square, Downtown Seattle Association, and Union Gospel Mission
- Ongoing outreach through final design and construction
- Project [webpage](#) and email list



From the entire SDOT/KCM Team:
Thank you!