



06.13.2013 DESIGN REVIEW RECOMMENDATION MEETING #2  
3829 CALIFORNIA AVE SW / DPD PROJECT #3013307

**caron**



Address : 3829 California Avenue SW  
 DPD Project Number : 3013307  
 Developer: Sergey Savchuk  
 Applicant: Caron Architecture  
 Contact: Scott Jeffries, Caron Architecture  
 Zoning: Lowrise 3 (LR-3)

The project is a 3-story apartment building containing 29 residential units and associated under-building parking. 29 parking stalls have been provided, with 20 below grade and 9 at grade. The existing 1-story apartments and garages on the site will be demolished. The building height and scale and are contextual to the adjacent multifamily and commercial buildings on California Ave.

Project History:

The Early Design Guidance meeting was held on June 28, 2012.  
 The Master Use Permit drawings were submitted on August 1, 2012.  
 The Design Review Recommendation #1 was held on April 11, 2013

Site Plan features:

- Residential building consistent with neighborhood character and scale
- 3 story height is consistent with recent multi-family developments in the area
- Main residential pedestrian entry at northeast corner of building enhances the relationship with the California Ave SW and SW Bradford St intersection
- Landscape plantings provided at the berm along California Avenue SW and at patio areas to the south and west sides of the building to hide the under-building parking
- A majority of the parking is provided below grade to minimize impact of surface parking
- Parking and trash pickup access from alley
- Existing street trees on California Ave SW will be retained

Amenities:

- Large common roof deck area with green roof, landscaping, and p-patch planters
- Private patios at 1st floor units
- Bicycle storage provided in parking garage

**DEVELOPMENT STATISTICS SUMMARY**

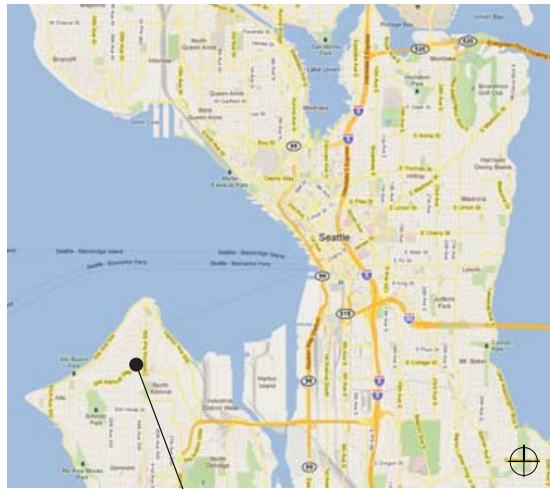
Lot Size	10,554 SF
FAR	1.5
Allowable FAR	<b>15,831 SF</b>
Proposed FAR	<b>15,113 SF</b>
Parking Stalls	29

	FAR SF	Units
Roof	348	0
3rd Floor	4,896	10
2nd Floor	5,085	11
Ground Floor	4,784	8
<b>Total</b>	<b>15,113</b>	<b>29</b>

PROJECT DESCRIPTION

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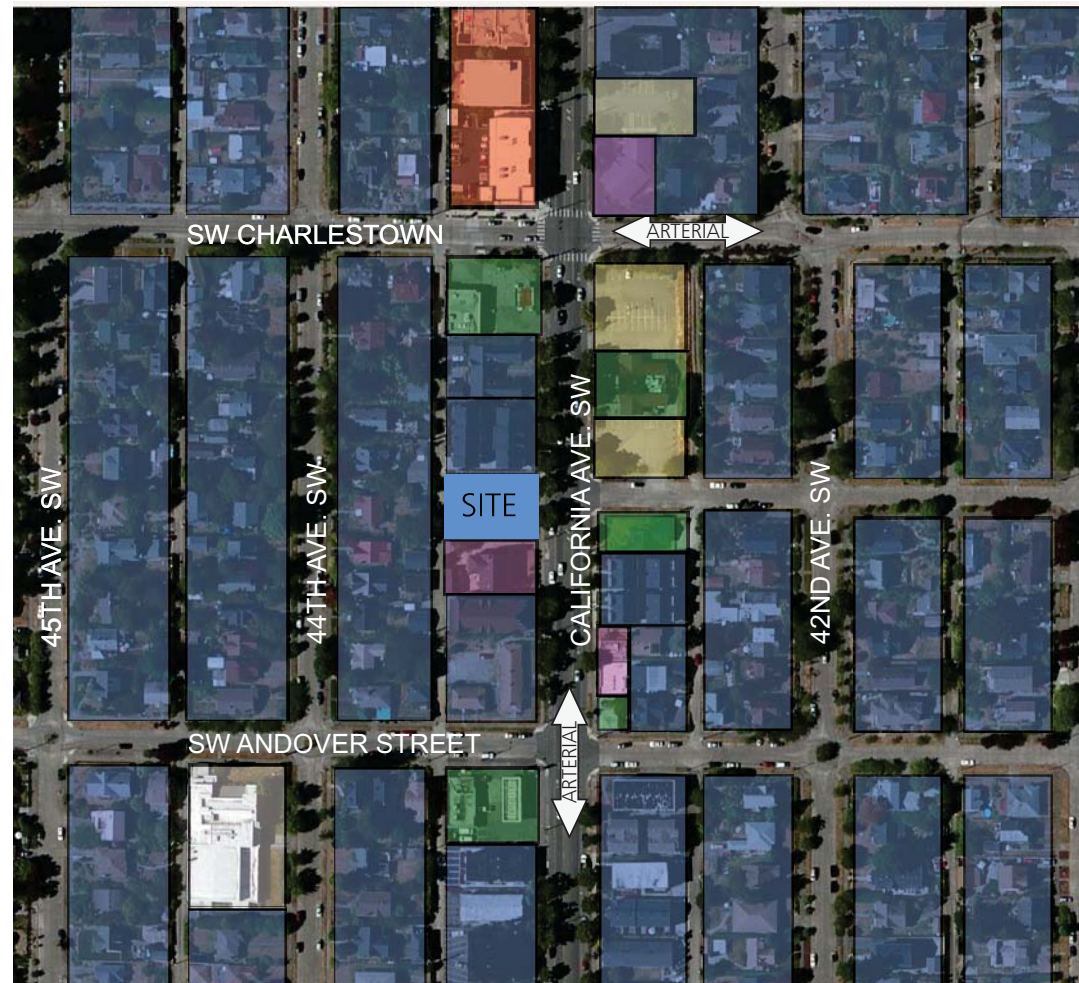


WEST SEATTLE



Located in the Genesee area of West Seattle, the site is located on the west side of California Avenue in a commercial corridor and is zoned LR-3 RC. The neighborhood is composed of single and multi-family residences, retail, office, mixed use and restaurant uses. A church is located approximately two blocks to the southwest. Most of the adjacent structures are low to mid-rise and were built in the last half of the 20th century. Many of the surrounding single family homes date from 1930's to 1940's.

The surrounding neighborhood is predominantly zoned SF-5000, with NC1-3 and LR3-RC zones to the north and south along California Ave SW. Arterials in the immediate vicinity are SW Charlestown Street and California Ave SW.



**VICINITY LEGEND**

- MIXED USE
- RETAIL
- OFFICE
- RESIDENTIAL
- SURFACE PARKING
- CHURCH



**ZONING LEGEND**

- SF 5000 Single Family
- NC1 Neighborhood Commercial
- LR3 Low Rise 3

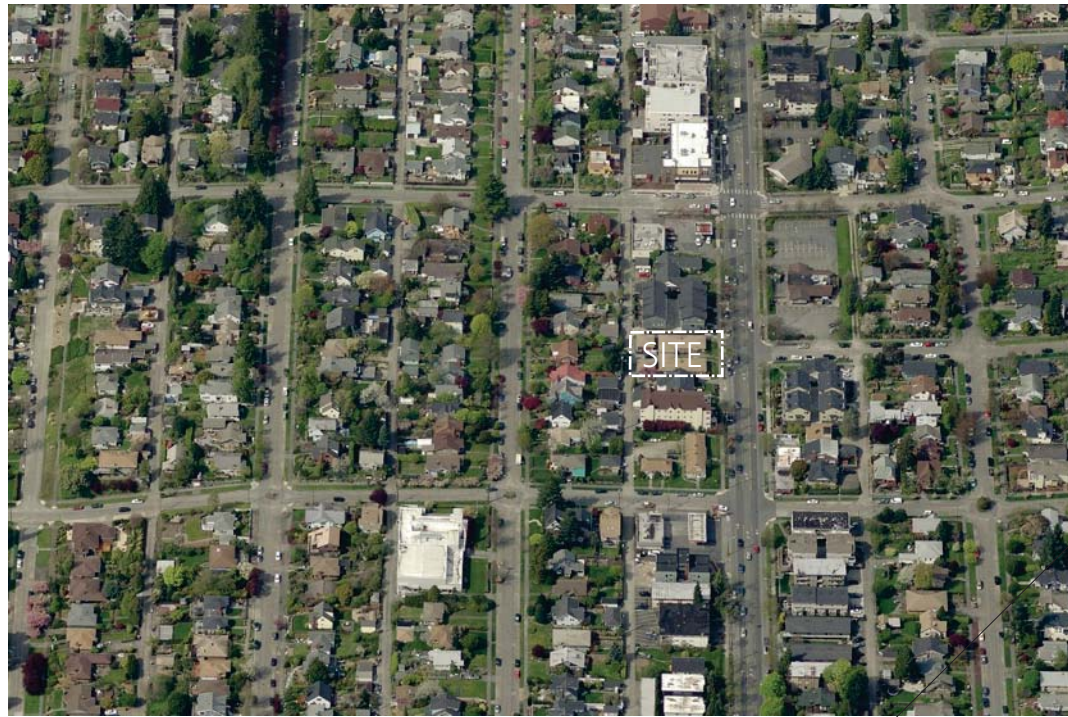
VICINITY MAP

ZONING MAP

NEIGHBORHOOD ANALYSIS

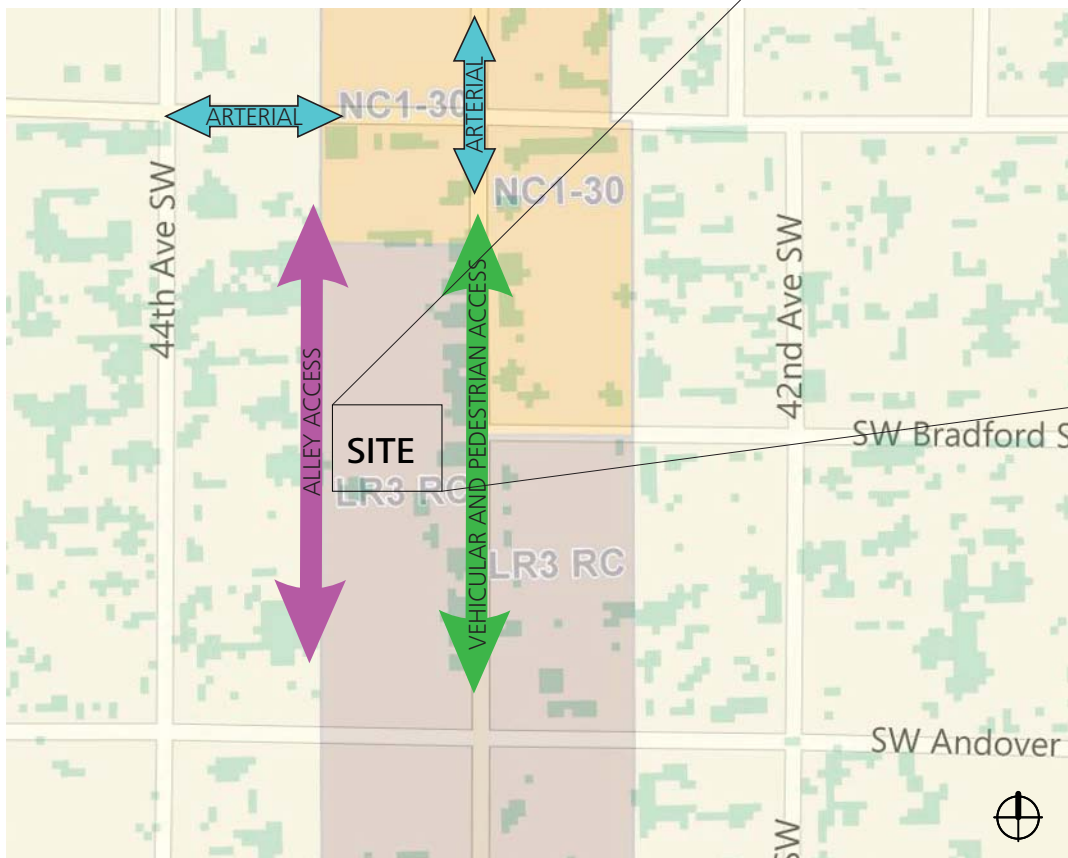
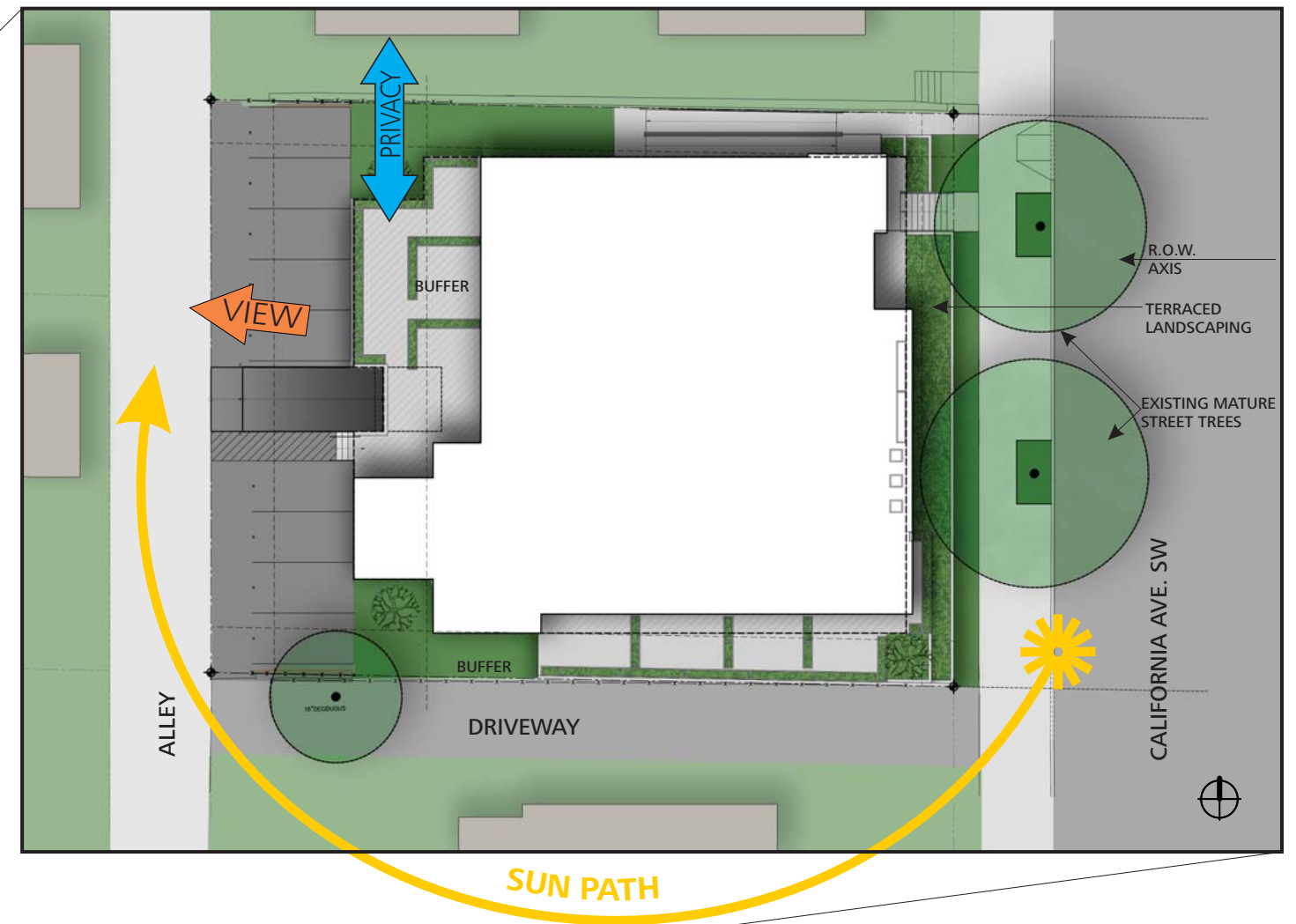
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SITE AERIAL

SITE PLAN



SITE ANALYSIS

SITE CONDITIONS

This site is currently built-out with two single story apartment buildings. It has very little slope and is approximately 24 inches above the California Ave SW right-of-way. All views are territorial looking over the neighborhood, with the exception of a potential view of the Olympic Mountains or a water view from the proposed roof top amenity area. Given the low height of adjacent buildings, the proposed apartment will not block views. RC zoning allows for potential commercial use, but this section of California is predominately residential in nature and the client selected not to provide any commercial use.

The urban tree canopy is fairly sporadic in the area, but this section of California Avenue has large street trees on west side of California Ave SW. The majority of the tree canopy is located in the single family areas.



NEIGHBORHOOD PHOTOS

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1. CALIFORNIA AVE SW LOOKING SOUTH



2. LOOKING WEST FROM CALIFORNIA AVE SW



3. CALIFORNIA AVE SW LOOKING NORTH



4. ALLEY LOOKING SOUTH



5. ALLEY LOOKING NORTH

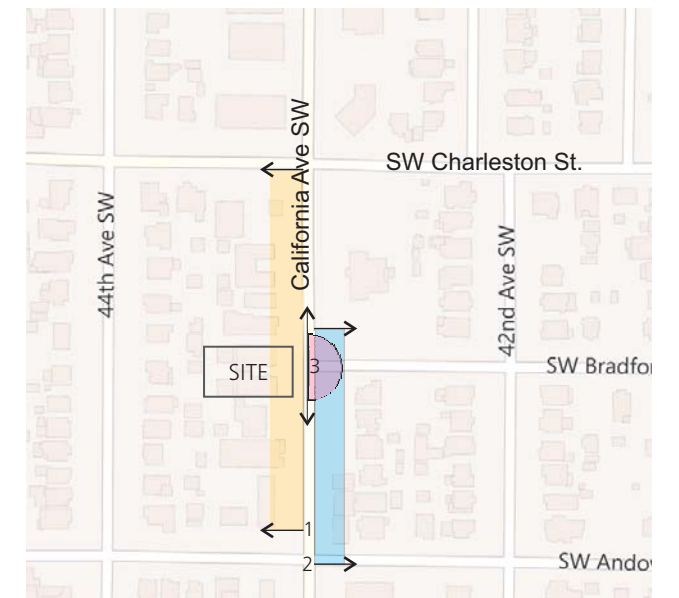
SITE PHOTOS

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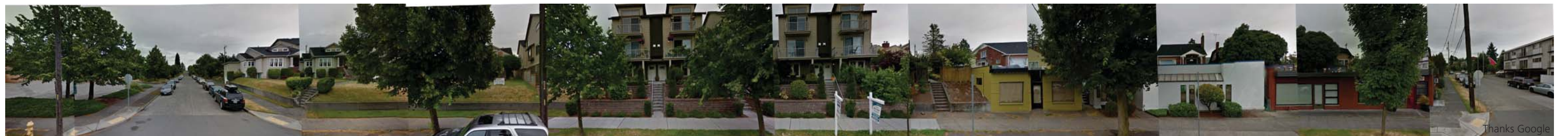


1 - STREET ELEVATION: CALIFORNIA AVE SW LOOKING WEST

- Common Design Themes :
- Mixture of gable and flat roofs
  - Variety of siding choices
  - Low rise
  - Elevated above street level



2 - INTERSECTION OF SW BRADFORD ST & CALIFORNIA AVE SW



3 - STREET ELEVATION: CALIFORNIA AVE SW LOOKING EAST

SITE PHOTOS

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**RESPONSE TO DRB RECOMMENDATION MEETING FOR DRB #2**

- Retaining wall added and berm slope reduced to avoid soil erosion onto sidewalk
- Landscaping in SW corner terraced
- Increased glazing on main floor facing California Ave SW, unit opens toward street
- Addition and extension of private patio spaces at street elevation
- Elevator penthouse color and material matches surrounding massing.
- Added joint lines in panels at street level, further articulating facade and creating more human scale at and visual interest at street level.



**ORIGINAL PROPOSAL AT EDG**

- Ramp proposed as a design element along street front
- Centralized entry lobby
- Vertical white volume extends to grade
- Extension of bermed landscaping to hide concrete wall of under-building parking



**RESPONSE TO EARLY DESIGN GUIDANCE MEETING, DRB #1**

- Building lowered into site
- Pedestrian ramp relocated to north side of building
- Entry lobby relocated to NE corner
- Developed massing of white volume, creating entry overhang and focal point

CALIFORNIA AVENUE ELEVATION LOOKING SOUTHWEST

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**RESPONSE TO DRB RECOMMENDATION MEETING FOR DRB #2**

- Redesign of SE corner to further activate the street. SE corner unit has increased glazing and corner unit now opens on to a street facing patio space.
- Neighbor's existing fence obscures retaining wall
- Hanging planters removed, planters added at patios along south facade
- Extension of private patios on south side to property line.
- Addition of planters along street facade.
- Added joint lines in panels at street level, further articulating facade and creating more human scale at and visual interest at street level.
- Addition of corner tree and landscaping to soften corner.



**ORIGINAL PROPOSAL AT EDG**

- High windows at SE corner unit along street
- Ramp proposed as a design element along street front
- Bermed landscape to obscure parking garage and reduce exposed concrete



**RESPONSE TO EARLY DESIGN GUIDANCE MEETING, DRB #1**

- Pedestrian ramp relocated to east side of building, further engaging street
- Southern retaining wall pulled back and hanging planters installed to hide retaining wall in response to concerned neighbor

CALIFORNIA AVENUE ELEVATION LOOKING NORTHWEST

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**RESPONSE TO DRB RECOMMENDATION MEETING FOR DRB #2**

- Facade length departure eliminated on south facade
- Glazing added to reduce blank wall at third floor step back
- Hanging planters removed at stair, 5' tall metal screen added for privacy
- Material, color and textural change on massing in SW corner and fire wall along exterior stair to break massing and bring street facade materials around to rear of building
- Increased landscaping at SW building corner
- Parking screening at property lines use building facade materials, extending project's visual reach to alleyway.



**ORIGINAL PROPOSAL AT EDG**

- Large glazing facing alley creates "eyes on the street" principle
- Exterior stair as an element, similar to fire escape
- Majority of massing pushed back from SF neighbors



**RESPONSE TO EARLY DESIGN GUIDANCE MEETING, DRB #1**

- Reduced height and extension of exterior stair
- Reduced height of SW corner massing
- West facing glazing reduced in response to privacy concerns for SF neighbors
- Stair moved away from SF zone, hanging planters added in response to privacy concerns

ALLEY ELEVATION LOOKING NORTHEAST

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**RESPONSE TO DRB RECOMMENDATION MEETING FOR DRB #2**

- Material, color and textural change on massing in SW corner and fire wall along exterior stair.
- Parking screening at property lines use building facade materials, extending project's visual reach to alleyway.
- Further development of exterior stair and railings
- Addition of trees and landscaping to soften facade
- Facade length departure eliminated on south facade
- Hanging planters removed at stair, 5' tall metal screen added for privacy
- Material, color and textural change on massing in SW corner and fire wall along exterior stair to break massing and bring street facade materials around to rear of building



**ORIGINAL PROPOSAL AT EDG**

- Exterior stair with green screen and hanging planters



**RESPONSE TO EARLY DESIGN GUIDANCE MEETING, DRB #1**

- Stair height lowered and pushed in toward the building, accommodating setback requirements. Planters wrapped around two sides of stair
- Addition of Juliet balconies
- Cutout at NW corner of patio space

ALLEY ELEVATION LOOKING SOUTHEAST

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**RESPONSE TO DRB RECOMMENDATION MEETING FOR DRB #2**

- Further development of rooftop landscaping
- Removal of lower green roof area due to structural concerns
- P patch planters and roof deck pulled away from neighboring townhomes



**ORIGINAL PROPOSAL AT EDG**

- Centralized stair and elevator landing
- Four planter boxes for gardening



**RESPONSE TO EARLY DESIGN GUIDANCE MEETING, DRB #1**

- Development of rooftop landscaping
- Elevator and stair landings pushed to east side of building

ROOF DECK LOOKING SOUTHEAST

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OPTION 2

06.13.2013 DESIGN REVIEW RECOMMENDATION MEETING #2  
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DESIGN REVIEW GUIDELINE	DRB #1 COMMENT	ARCHITECT'S RESPONSE
<b>A-1: Responding to Site Characteristics</b> The siting of buildings should respond to specific site conditions and opportunities such as non-rectangular lots, location on prominent intersections, unusual topography, significant vegetation and views of other natural features.	The Board suggested terracing the landscaping along California Ave SW instead of the proposed berm. They were concerned a newly planted berm has the potential for soil washing unto the sidewalk.	The bermed landscape area has been redesigned so that the bermed area has been reduced in size and retained by a low concrete wall at the property line to avoid any soil erosion. As such, the slope of the berm has been reduced to further minimize any soil erosion. The southeast portion of the landscape area has been terraced and now includes a private unit patio.
<b>A-2: Streetscape Compatibility</b> The citing of buildings should acknowledge and reinforce the existing desirable spatial characteristics of the right-of-way.	The Board thought that the proposed east facade facing California Ave SW was not responding well to the streetscape. They suggested bringing the proposed south facing patios, around the corner and along the southeast portion of the façade. Other suggestions made by the Board were treating the street facing facade to appear more as townhouses, and omitting the setbacks at the first floor level.	The ground floor of the eastern facade has been revisited to provide more glazing and an east facing private unit patio. The siding has been divided further and landscape planters break up the blank wall sections. The landscape berm has also been terraced at the southeast portion to give the corner more visual interest.
<b>A-5: Respect for Adjacent Sites</b> Buildings should respect adjacent properties by being located on their sites to minimize disruption of the privacy and outdoor activities of residents in adjacent buildings.	The Board indicated their concern that the proposed solution of planters along the south retaining wall, and west facing open stairwell, will most likely not work and the vegetation will die. The Board suggested providing a green wall or screening with ground based landscaping at the edge of the south facing patios. This would provide privacy. The Board also suggested removing a surface parking stall to provide trees and landscaping along the alley. See Guidelines A-7.	The existing fence along the south property line appears to belong to the neighbor to the south, and as such the fence will remain. The hanging planters have been removed and the southerly retaining wall has pulled to within six inches of the property line. Planters have been provided at the patio level and the 42 inch high guardrail has been designed so the plants may climb up it to create more of a green screen as well as weeping down to obscure the retaining wall. The top of the new guards is approximately the same as the existing six foot tall neighboring fence.  The hanging planters at the exterior stair have been removed and replaced with a five foot tall metal screen for privacy.
<b>A-7: Residential Open Space</b> Buildings should respect adjacent properties by being located on their sites to minimize disruption of the privacy and outdoor activities of residents in adjacent buildings.	The Board suggested bringing the patio proposed along the south elevation, around the corner along the southeast portion of the California Ave SW facing facade. The Board suggested the patios be designed to act as an "outdoor room" for the units and to provide more privacy for both project residents and current and future users of development to the south. See Guideline A-5.	A patio has been added at the southeastern corner of the building. The 42 inch high guardrail at the edge of of the patio has been designed so the plants may climb up it to create more of a green screen and down to hide the retaining wall. The patios are small spaces and the design team feels that if they are any taller the patio will start to feel jail like and uninviting. Taller screens also reduce visibility to the street and hence CPTED provisions.
<b>A-8: Parking and Vehicle Access</b> Siting should minimize the impact of automobile parking and driveways on the pedestrian environment, adjacent properties, and pedestrian safety.	The Board the Board suggested removing a surface parking stall to provide landscaping along the alley. See Guideline A-5.	Due to the redesign of the south facade to remove the facade length departure request, the utilities were required to be relocated in the building and a parking stall in the garage was lost. Subsequently, the project is at the minimum required parking and there is not an extra surface stall which may be converted to a planting area.
<b>C-2: Architectural Concept and Consistency</b> Building design elements, details and massing should create a well-proportioned and unified building form and exhibit an overall architectural concept. Buildings should exhibit form and features identifying the functions within the building. In general, the roofline or top of the structure should be clearly distinguished from its facade walls.	The Board suggested consistency in materials along the facades, especially the south and west elevations. They also suggested changing the lighter color of the elevator penthouse to blend in with the lower roof penthouse structure, and providing landscaping along the blank wall of the west facing facade at the proposed second level green roof.	The Southwest corner of the building has been redesigned to introduce a material and texture change along the alley. The firewall separating the exterior stair from the west ground level roof deck has also had its material and color changed. The redesign of the southwest corner has introduced more glazing at the third floor facing west, and as such reduces the blank wall at the third floor. The green roof at the second floor roof has been removed due to structural considerations. The project still provides more green roof on the main roof than is required by Code.
<b>C-3: Human Scale</b> The design of new buildings should incorporate architectural features, elements, and details to achieve a good human scale.	The Board suggested that the design activate the street facing east facade with a better human scale, including using smaller cementious panels then the propose 4' x 8' size.	As previously noted, the southeast corner has been redesigned to activate the street more. The fibercement panels have been reduced from a four foot tall to a two foot tall panel, with varying lengths.
<b>C-4: Exterior Finish Materials</b> Building exteriors should be constructed of durable and maintainable materials that are attractive even when viewed up close. Materials that have texture, pattern, or lend themselves to a high quality of detailing are encouraged.	The Board suggested using higher quality materials at grade and the street facing corners. The Board would like to see the proposed Juliette balconies in more detail to understand how they will work. They also suggested changing the lighter color of the elevator penthouse to blend in with the lower roof penthouse structure.	The material at the pedestrian lobby corner presented at the DRB recommendation meeting is blackened steel, and has not changed. The design team studied bringing the blackened steel material to the ground floor at the southeast corner, but it had a negative effect on the massing, making the entirety of the ground floor quite dark. We feel that keeping the light colored material at the patios is the appropriate move. The elevator penthouse has been redesigned and the siding material now matches the rest of the roof penthouse material.

RESPONSE TO DESIGN REVIEW RECOMMENDATION MEETING #1

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DESIGN REVIEW GUIDELINE	DRB #1 COMMENT	ARCHITECT'S RESPONSE
<p><b>D-2: Blank Walls</b> Buildings should avoid large blank walls facing the street, especially near sidewalks. Where blank walls are unavoidable they should receive design treatment to increase pedestrian comfort and interest.</p>	<p>The Board spent much time discussing the California Ave SW elevation. They indicated their desire to see more glazing along the street facing elevation to activate the facade (eyes on the street) and provide a better human scale. This could include more detailing, and incorporating quality materials at the facade corners and the first floor level, as well as an increased amount of glazing.</p>	<p>The glazing along California Ave SW at the ground floor has been increased and the corner unit now opens to the street. A patio now faces the street at the southeast corner as well. The materials have been redesigned as well, per the Guideline C-3 response. The landscaping has also been revised to bring more interest.</p>
<p><b>D-3: Retaining Walls</b> Retaining walls near a public sidewalk that extend higher than eye level should be avoided where possible. Where higher retaining walls are unavoidable, they should be designed to reduce their impact on pedestrian comfort and to increase the visual interest along the streetscapes.</p>	<p>The Board expressed their concerned that the planters proposed along the south retaining wall will most likely not work and the vegetation will die. The Board suggested providing a green wall or screening with ground based landscaping at the edge of the south facing patios. This could screen the retaining wall and provide privacy.</p>	<p>The existing fence along the south property line appears to belong to the neighbor to the south, and as such the fence will remain. The hanging planters have been removed and the southerly retaining wall has pulled to within six inches of the property line. Planters have been provided at the patio level and the 42 inch high guardrail has been designed so the plants may climb up it to create more of a green screen as well as weeping down to obscure the retaining wall. The top of the new guards is approximately the same as the existing six foot tall neighboring fence.</p>
<p><b>D-5: Visual Impacts of Parking Structure</b> The visibility of all at-grade parking structures or accessory parking garages should be minimized. The parking portion of a structure should be architecturally compatible with the rest of the structure and streetscape. Open parking spaces and carports should be screened from the street and adjacent properties.</p>	<p>The Board suggested removing a surface parking space and providing trees and landscaping to soften the impact of the parking.</p>	<p>Due to the redesign of the south facade to remove the facade length departure request, the utilities were required to be relocated in the building and a parking stall in the garage was lost. Subsequently, the project is at the minimum required parking and there is not an extra surface stall which may be converted to a planting area.</p>
<p><b>D-12: Residential Entries and Transitions</b> For residential projects in commercial zones, the space between the residential entry and the sidewalk should provide security and privacy for residents and a visually interesting street front for pedestrians. Residential buildings should enhance the character of the streetscape with small gardens, stoops and other elements that work to create a transition between the public sidewalk and private entry.</p>	<p>Special attention should be paid to creating a pleasant and inviting transition between the sidewalk and the residential entry.</p>	<p>Landscaping has been added between the front façade and the sidewalk, and the accessible pedestrian ramp has been shifted to the northeast corner of the site to soften the relationship between the building and the sidewalk. The building has been reduced in height by one foot, minimizing the elevation change between the sidewalk elevation and the first floor elevation.</p>
<p><b>E-2: Landscaping to Enhance the Building and/or Site</b> Landscaping, including living plant material, special pavements, trellises, screen walls, planters, site furniture and similar features should be appropriately incorporated into the design to enhance the project.</p>	<p>The Board expressed their concern that the proposal lacked a viable, and consistent landscape concept. They would like to see a readable complete landscape plan. Board suggestions, noted in many of the above design guidelines include:</p> <ul style="list-style-type: none"> <li>• Terracing the landscaping along California Ave SW instead of a berm. They are concerned a newly planted berm has the potential for soil washing unto the sidewalk.</li> <li>• Providing screening with ground based landscaping at the edge of the south facing patios.</li> <li>• Landscaping along the blank wall of the west facing facade at the proposed green roof.</li> <li>• Removal of a surface parking stall to provide landscaping at the alley.</li> </ul>	<p>A new landscape plan has been included.</p> <ul style="list-style-type: none"> <li>• The bermed landscape area has been redesigned so that the bermed area has been reduced in size and retained by a low concrete wall at the property line to avoid any soil erosion. As such, the slope of the berm has been reduced to further minimize any soil erosion. The southeast portion of the landscape area has been terraced and now includes a private unit patio.</li> <li>• The existing fence along the south property line appears to belong to the neighbor to the south, and as such the fence will remain. The hanging planters have been removed and the southerly retaining wall has pulled to within six inches of the property line. Planters have been provided at the patio level and the 42 inch high guardrail has been designed so the plants may climb up it to create more of a green screen as well as weeping down to obscure the retaining wall. The top of the new guards is approximately the same as the existing six foot tall neighboring fence.</li> <li>• The redesign of the southwest corner has introduced more glazing at the third floor, facing west, and as such reduces the blank wall at the third floor. The green roof at the second floor roof has been removed due to structural considerations. The project still provides more green roof on the main roof than is required by Code.</li> <li>• Due to the redesign of the south facade to remove the facade length departure request, the utilities were required to be relocated in the building and a parking stall in the garage was lost. Subsequently, the project is at the minimum required parking and there is not an extra surface stall which may be converted to a planting area.</li> </ul>

DESIGN REVIEW GUIDELINE	EDG COMMENT	ARCHITECT'S RESPONSE
<b>A-1: Responding to Site Characteristics</b> The siting of buildings should respond to specific site conditions and opportunities such as non-rectangular lots, location on prominent intersections, unusual topography, significant vegetation and views of other natural features.	Address the awkward sequence of entry, including the accessible ramp, on California Ave SW created by the heightened berm, and "push the building down" as far as possible.	Access to the parking was re-evaluated and the building was lowered by approximately one foot. The drop is limited by the limited length of the vehicular ramp and overhead clearance requirements. In addition, the existing berm along California Ave SW is to be retained and added on-top of in order to reduce the impact of the partially buried story. The ADA pedestrian ramp has been relocated along the northerly property line to minimize its dominance, allowing for more landscaping and a more prominent pedestrian entry along the street front.
<b>A-2: Streetscape Compatibility</b> The citing of buildings should acknowledge and reinforce the existing desirable spatial characteristics of the right-of-way.	The choreography of pedestrian movements needs more exploration and attention, as does the integration of the entry and ramp with the location of other elements of the structure.	In addition to relocating the ADA ramp, the building was redesigned to place the pedestrian lobby in the northwest corner of the building, which is more on-axis with the pedestrian curb cut to California Ave SW, as well as the sidewalk alignment of SW Bradford Street, creating a prominent street entry corner to the street experience.
<b>A-3: Entrance Visible from the Street</b> Entries should be clearly identifiable and visible from the street.	The entry stairs, entry, lobby, and accessible ramp should be more integrally located and apportioned.	The entry sequence is now directly in the prominent corner element. The ADA pedestrian ramp was relocated in the side setback and made much less a feature and works with existing grades.
<b>A-5: Respect for Adjacent Sites</b> Buildings should respect adjacent properties by being located on their sites to minimize disruption of the privacy and outdoor activities of residents in adjacent buildings.	Respect for adjacent properties needs to be revised in the following design elements: the location of windows on the north façade, the blank wall along the south property edge, the open stairwell and balcony overlooks that face the single family neighbors to the west.	The site retaining wall along the south property line has been pulled back 18 inches to allow for a hanging planter with weeping vegetation, which will cover the concrete retaining wall. A green-screen has been added along the west exposure of the open stairway at the rear of the building to provide privacy for the adjacent single family neighbors. Windows along the north façade have been minimized and placed as high within the units as possible to provide maximum privacy for all residents and adjacent townhomes. The 3rd floor unit at the southwest corner of the building has been eliminated, and the large windows at the 1st and 2nd floor units at the southwest corner have been reoriented to the south, away from the single family neighbors and towards the existing commercial building.
<b>A-7: Residential Open Space</b> Buildings should respect adjacent properties by being located on their sites to minimize disruption of the privacy and outdoor activities of residents in adjacent buildings.	Address the choice of size and configuration of open spaces provided for the ground floor units along the south property line.	Common open spaces have been located at the first floor level on the east, north, and west sides of the building, orienting them away from the adjacent residential building to the north and providing them with opportunities for direct sunlight. The common roof deck is also located in the middle of the roof to minimize conflict with the neighbors. Private patios are located on south west podium.
<b>A-8: Parking and Vehicle Access</b> Siting should minimize the impact of automobile parking and driveways on the pedestrian environment, adjacent properties, and pedestrian safety.	Provide a better design solution to the basement parking garage and its effect on the height of the entry/lobby level, sidewalk, and accessible pathway structure in front.	The garage entry ramp has been redesigned to allow a one foot reduction in the height of the garage lid from the design presented at EDG, which allowed the first floor height to be reduced in relation to the sidewalk and reduced the length of the accessible pathway. A DEPARTURE IS REQUIRED TO MAKE THE RAMP STEEPER.
<b>C-2: Architectural Concept and Consistency</b> Building design elements, details and massing should create a well-proportioned and unified building form and exhibit an overall architectural concept. Buildings should exhibit form and features identifying the functions within the building. In general, the roofline or top of the structure should be clearly distinguished from its facade walls.	Functions should be conveyed through various articulations of the façades.	Composition, massing, and exterior materials and colors are indicated on the building elevations. The massing reflects vertical elements at the main pedestrian entrance and elevator tower, and horizontal elements at the units. This places an emphasis on the entry by differentiating it from the body and reduces the massing elements to a more pedestrian scale.
<b>C-3: Human Scale</b> The design of new buildings should incorporate architectural features, elements, and details to achieve a good human scale.	Continue to break down the mass of the structure and modulate the façades through a variety of techniques.	The massing and composition have further been refined and broken by color and modulation, in addition to change of pattern and orientation of paneling to take into account the Board's comments and suggestions.
<b>C-4: Exterior Finish Materials</b> Building exteriors should be constructed of durable and maintainable materials that are attractive even when viewed up close. Materials that have texture, pattern, or lend themselves to a high quality of detailing are encouraged.	Consideration should be taken with attachments, detailing and the interface of materials. Of special interest is the treatment of the accessibility ramp.	Exterior materials are indicated on the building elevations. The ADA pedestrian ramp has been relocated to be much less prominent and as such the emphasis on the ramp material is diminished. The accessible ramp consists of architecturally exposed cast in place concrete with painted architectural steel railings. The pattern of the walking surfaces have been given texture to create interest and differentiate it from the surrounding hardscape.

RESPONSE TO EARLY DESIGN GUIDANCE MEETING

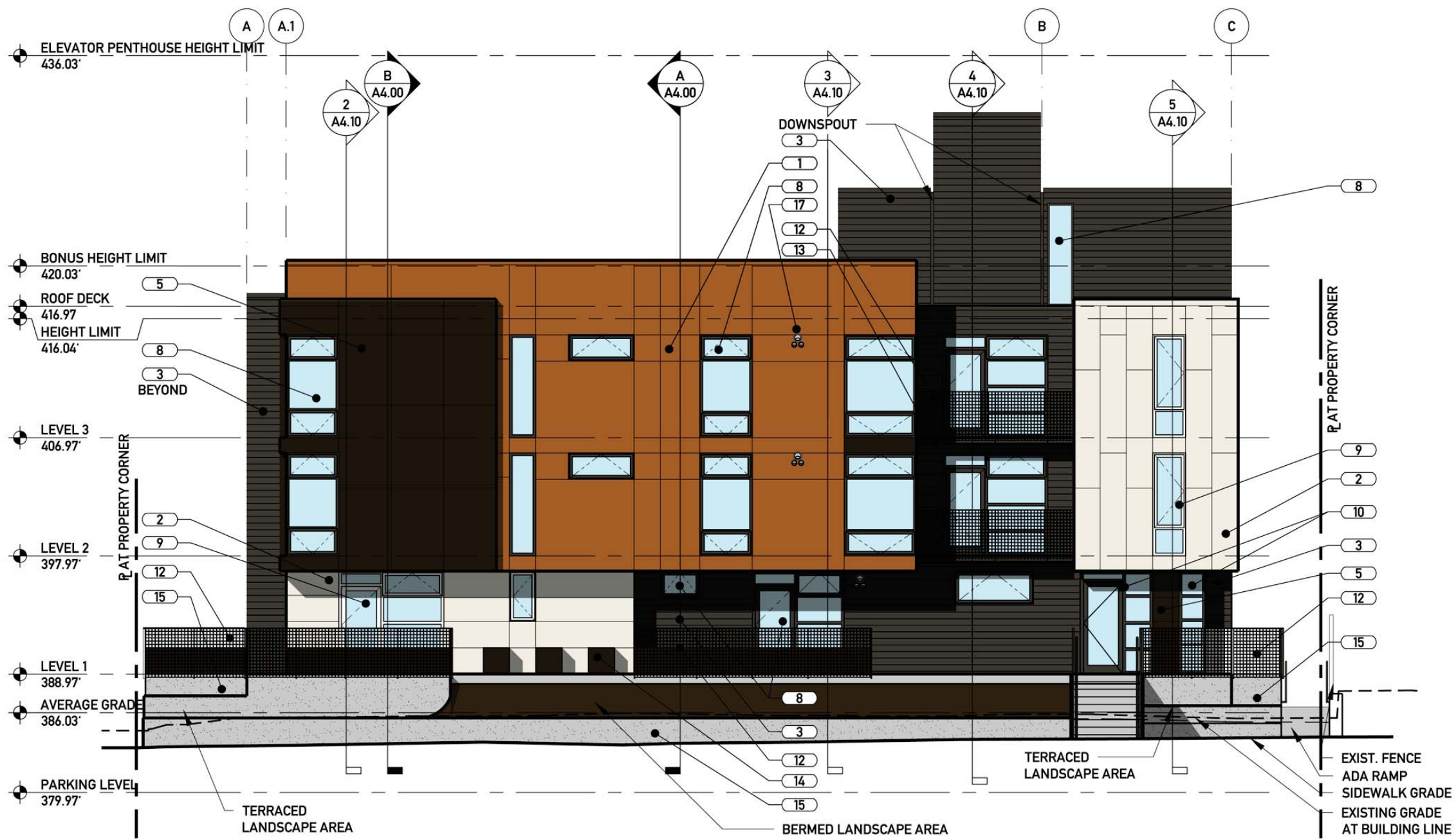
06.13.2013 DESIGN REVIEW RECOMMENDATION MEETING #2  
 3829 CALIFORNIA AVE SW / DPD PROJECT #3013307

DESIGN REVIEW GUIDELINE	EDG COMMENT	ARCHITECT'S RESPONSE
<b>D-1: Pedestrian Open Spaces and Entrances</b> Convenient and attractive access to the building's entry should be provided. To ensure comfort and security, paths and entry areas should be sufficiently lighted and entry areas should be protected from the weather. Opportunities for creating lively, pedestrian-oriented open space should be considered.	Address relocating the entry and lobby.	In addition to relocating the ADA pedestrian ramp, the building was redesigned to place the pedestrian lobby in the northeast corner of the building, which is more on-axis with the pedestrian curb cut to California Ave SW, as well as the sidewalk alignment of SW Bradford Street, allowing for a more cohesive pedestrian choreography and felicitous interplay to the street experience. The building overhang above allows for weather protection and downward directed lighting above the main entry.
<b>D-2: Blank Walls</b> Buildings should avoid large blank walls facing the street, especially near sidewalks. Where blank walls are unavoidable they should receive design treatment to increase pedestrian comfort and interest.	Address the blank wall running along the south property line.	The site retaining wall along the south property line has been pulled back 18 inches to allow for a hanging planter with weeping vegetation which will cover the concrete retaining wall. The parking garage lid has been reduced in height by 1 foot to minimize the height of the concrete retaining wall to a maximum height of approximately 4'-3" at the southeast corner and a typical height of 2'-6".
<b>D-3: Retaining Walls</b> Retaining walls near a public sidewalk that extend higher than eye level should be avoided where possible. Where higher retaining walls are unavoidable, they should be designed to reduce their impact on pedestrian comfort and to increase the visual interest along the streetscapes.	Address the wall along the south property line.	The site retaining wall along the south property line has been pulled back 18 inches to allow for a hanging planter with weeping vegetation which will cover the concrete retaining wall. The parking garage lid has been reduced in height by one foot to minimize the height of the concrete retaining wall to a maximum height of approximately 4'-3" at the southeast corner and a typical height of 2'-6".
<b>D-5: Visual Impacts of Parking Structure</b> The visibility of all at-grade parking structures or accessory parking garages should be minimized. The parking portion of a structure should be architecturally compatible with the rest of the structure and streetscape. Open parking spaces and carports should be screened from the street and adjacent properties.	Provide a better design solution to the basement parking garage and its effect on the height of the entry/lobby level, sidewalk, and accessible pathway structure in front.	The garage entry ramp has been redesigned to allow a 1 foot reduction in the height of the garage lid from the design presented at EDG, to minimize the "lift" of the building. The exterior parking at the alley is screened by wood privacy fences near the north and south property lines.
<b>D-12: Residential Entries and Transitions</b> For residential projects in commercial zones, the space between the residential entry and the sidewalk should provide security and privacy for residents and a visually interesting street front for pedestrians. Residential buildings should enhance the character of the streetscape with small gardens, stoops and other elements that work to create a transition between the public sidewalk and private entry.	Special attention should be paid to creating a pleasant and inviting transition between the sidewalk and the residential entry.	Landscaping has been added between the front façade and the sidewalk, and the accessible pedestrian ramp has been shifted to the northeast corner of the site to soften the relationship between the building and the sidewalk. The building has been reduced in height by one foot, minimizing the elevation change between the sidewalk elevation and the first floor elevation.
<b>E-3: Landscape Design to Address Special Site Conditions</b> The landscape design should take advantage of special on-site conditions such as high-bank front yards, steep slopes, view corridors, or existing significant trees and off-site conditions such as greenbelts, ravines, natural areas, and boulevards.	A major attempt should be made to soften the project along its southern property line. The project should present a pleasant face to the single family residences across the alley. Serious efforts should be made to integrate the open space areas of the front with the pedestrian path, entry, and ADA ramp.	The site retaining wall along the south property line has been pulled back 18 inches to allow for a hanging planter with weeping vegetation which will cover the concrete retaining wall. The relocation of the ramp allows for a large, inviting landscape area to present the building to the street and to minimize the impact of the under-building parking. Landscaping on the podium will provide both visual interest and privacy protection to and from the single family neighbors to the west.

REQUESTED DEPARTURE					
CODE SECTION	REQUIREMENT	PROVIDED	COMPLIES	DEPARTURE REQUEST	RATIONALE
23.54.030.D.3	Driveway slope for all uses. No portion of a driveway, whether located on a lot or on a right-of-way, shall exceed a slope of 15 percent, except as provided in this subsection 23.54.030.D.3....The Director may permit a driveway slope of more than 15 percent if it is found that: a. The topography or other special characteristic of the lot makes a 15 percent maximum driveway slope infeasible; b. The additional amount of slope permitted is the least amount necessary to accommodate the conditions of the lot; and c. The driveway is still useable as access to the lot.	20% maximum driveway slope	N	Allow a 20% maximum driveway slope with 10% crest and sag	The steeper ramp slope allows the first floor elevation to be set lower to make the project massing smaller and help to address the entry sequence from California Avenue. The board noted this under Design Guidelines A-1, <a href="#">Responding to Site Characteristics</a> and A-8, <a href="#">Parking and Vehicle Access</a> .

RESPONSE TO EARLY DESIGN GUIDANCE MEETING AND DEPARTURE REQUEST

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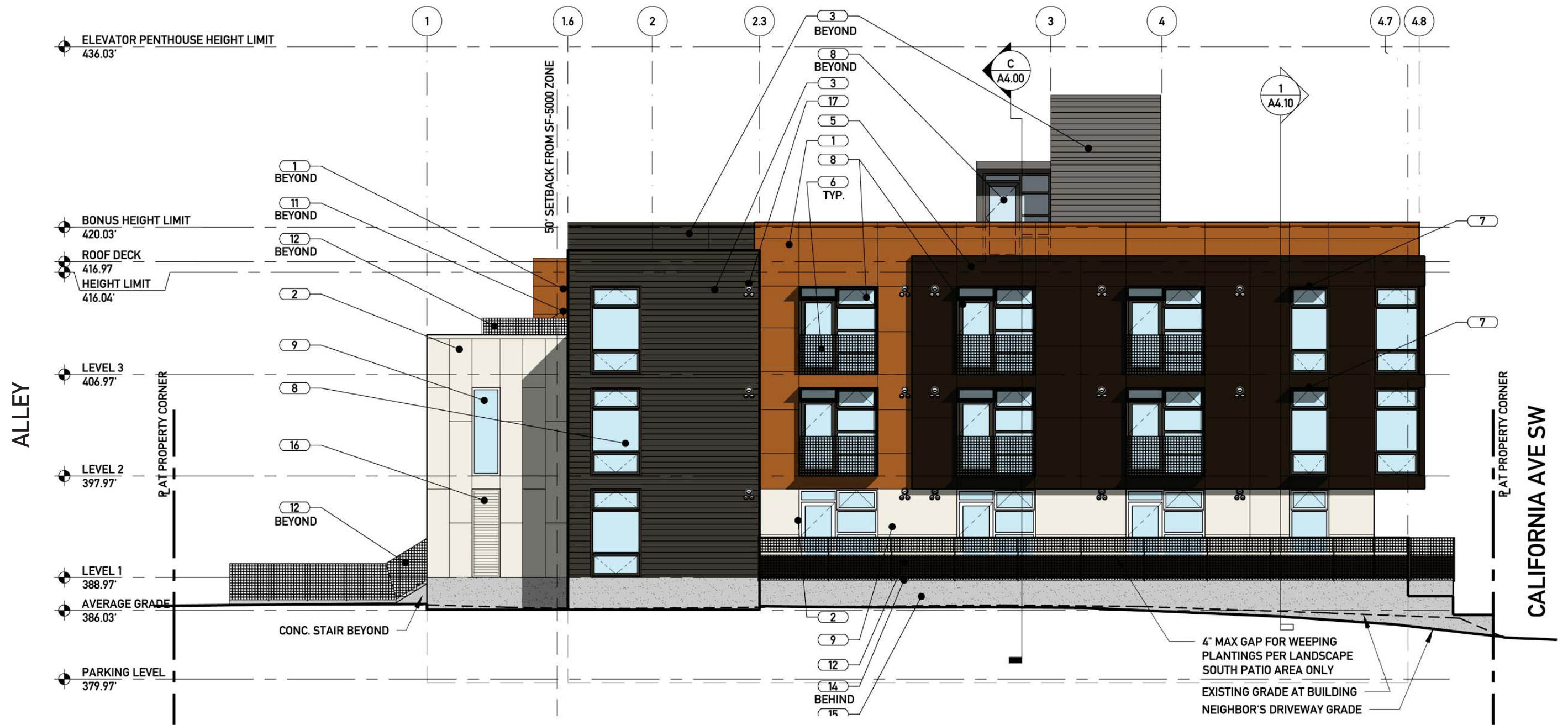
## MATERIALS SCHEDULE

1. FIBERCEMENT SIDING PANEL W/ REGLET  
 PANEL COLOR : SHERWIN WILLIAMS SW 6356 COPPER MOUNTAIN  
 PANEL SHEEN : FLAT  
 REGLET COLOR: SHERWIN WILLIAMS 6356 COPPER MOUNTAIN  
 REGLET SHEEN : FLAT
2. FIBERCEMENT SIDING PANEL  
 COLOR : SHERWIN WILLIAMS SW 7566 WESTHIGHLAND WHITE  
 SHEEN : FLAT  
 PANEL SHEEN : FLAT  
 REGLET COLOR: SHERWIN WILLIAMS 7566 WESTHIGHLAND WHITE  
 REGLET SHEEN : FLAT
3. HORIZONTAL FIBERCEMENT SIDING  
 COLOR : SHERWIN WILLIAMS SW 7675 SEALSKIN  
 SHEEN : FLAT
4. METAL FLASHING  
 COLOR : SIDING PANEL COLOR
5. BLACKENED SHEET STEEL OVER RAINSCREEN
6. SHEET STEEL SUNSCREEN W/ JULIET BALCONY SEE DETAIL 4/A4.20  
 COLOR : SHERWIN WILLIAMS SW 7675 SEALSKIN  
 SHEEN : FLAT
7. SHEET STEEL SUNSCREEN  
 COLOR : SHERWIN WILLIAMS SW 7675 SEALSKIN  
 SHEEN : FLAT
8. VINYL WINDOW / DOOR ASSEMBLY  
 COLOR : DARK GREY
9. VINYL WINDOW / DOOR ASSEMBLY  
 COLOR : WHITE
10. ALUMINUM CLAD WOOD WINDOW  
 COLOR : IRON ORE
11. EXTERIOR STEEL STAIR  
 COLOR : SW 7675 SEALSKIN  
 SHEEN : SEMI-GLOSS
12. STEEL RAILING W/ McNICOLS TECHNA 3155 MESH STEEL PANEL  
 SEE DET. 6 AND 7 A4.20  
 RAILING COLOR : SW 7675 SEALSKIN  
 RAIL SHEEN : SEMI-GLOSS  
 MESH PANEL COLOR : NATURAL STEEL
13. STEEL BALCONY  
 COLOR : SW 7675 SEALSKIN  
 SHEEN : SEMI-GLOSS
14. PRE-FABRICATED PLANTER  
 TOURNESOL WILSHIRE COLLECTION
15. CONCRETE RETAINING WALL
16. SELF DRAINING LOUVER W/ 2 1/2" GRILL SPACING  
 COLOR : SHERWIN WILLIAMS SW7566 WESTHIGHLAND WHITE  
 SHEEN : FLAT
17. SEI-HO ENVIRONMENTAL GRILL  
 MODEL: SFB  
 FINISH : ANODIZED ALUMINUM

CALIFORNIA AVE SW ELEVATION (WEST)

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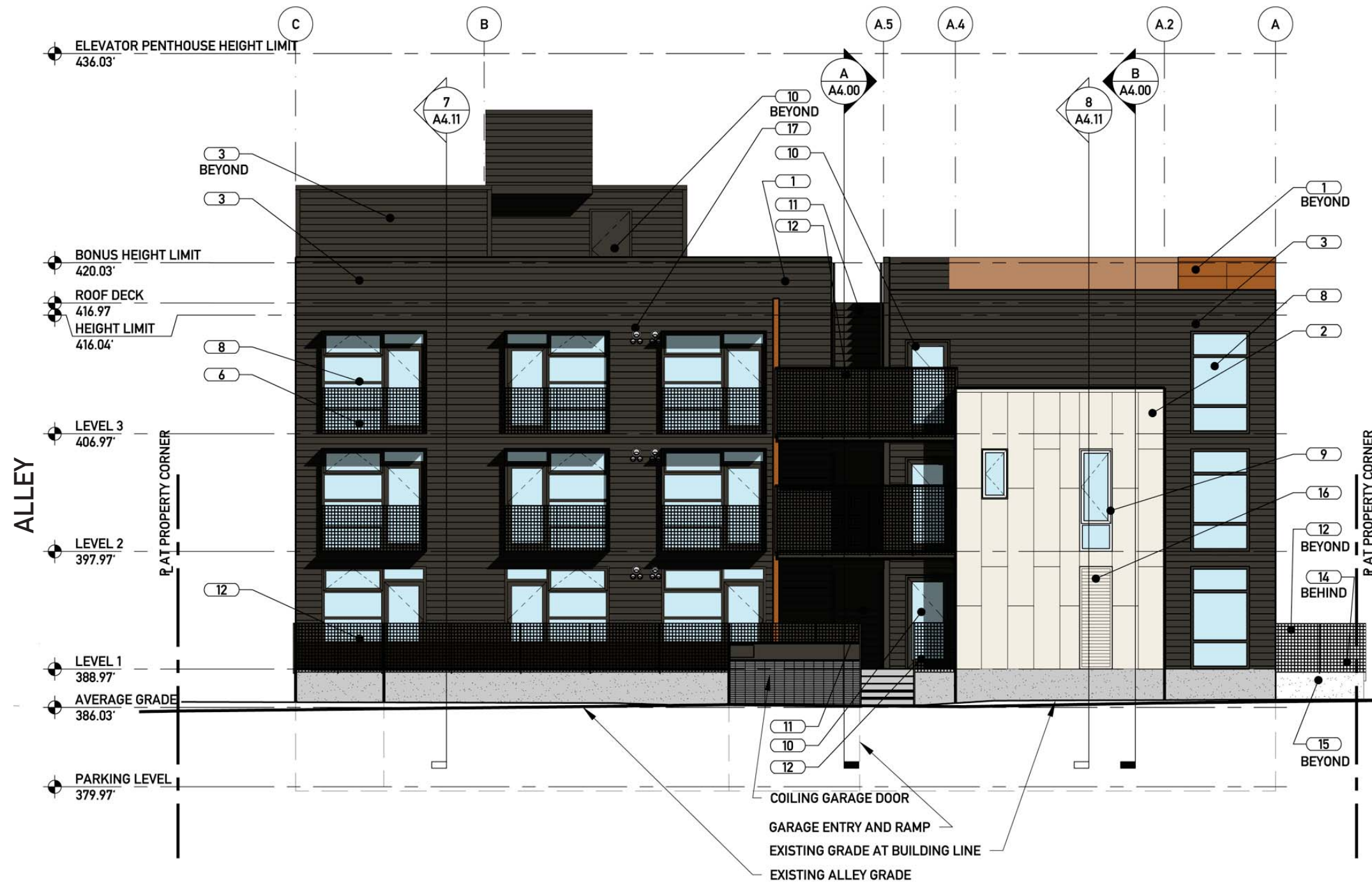




SOUTH ELEVATION

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 3829 CALIFORNIA AVE SW / DPD PROJECT #3013307





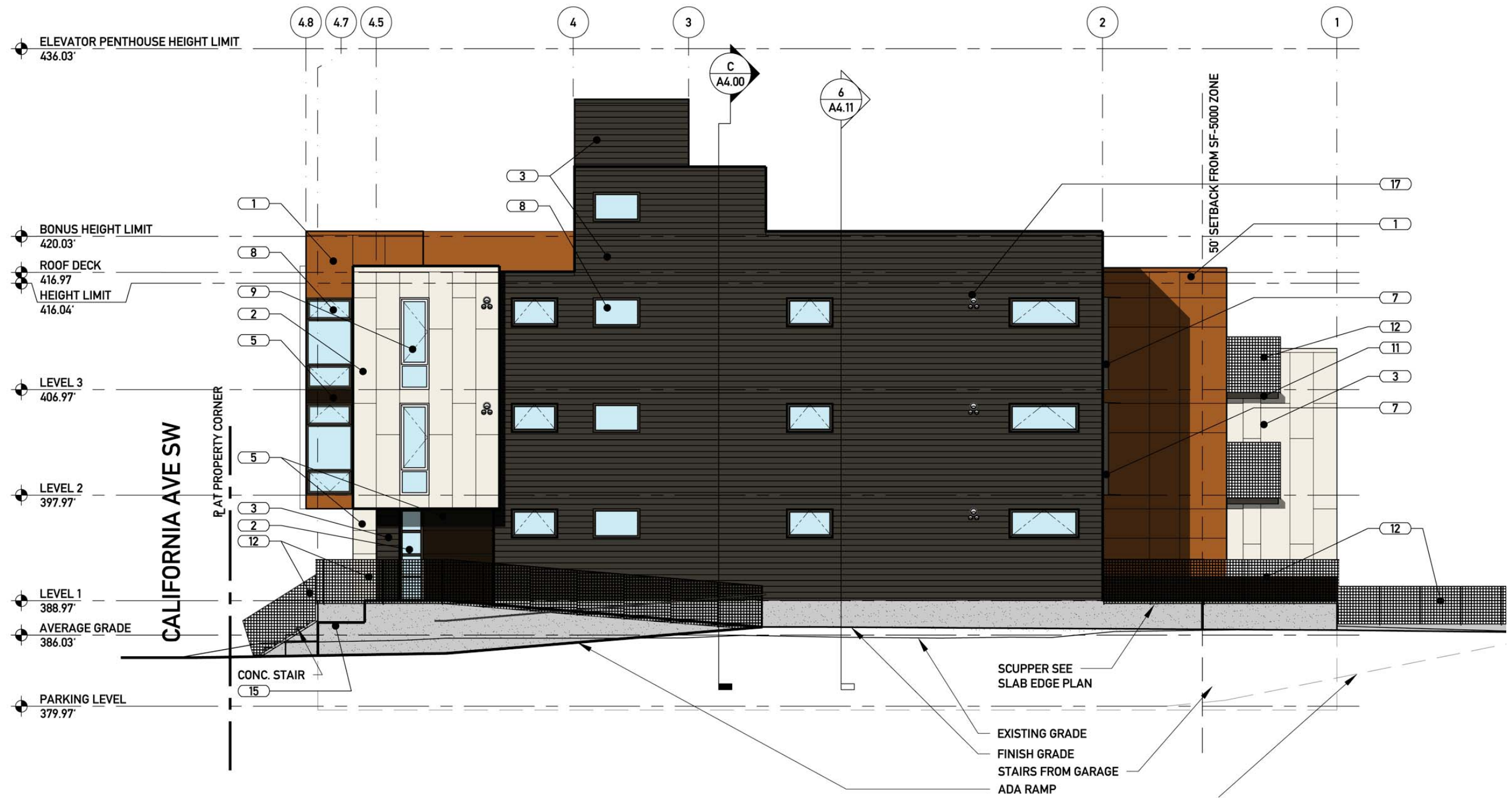
### MATERIALS SCHEDULE

1. FIBERCEMENT SIDING PANEL W/ REGLET  
 PANEL COLOR : SHERWIN WILLIAMS SW 6356 COPPER MOUNTAIN  
 PANEL SHEEN : FLAT  
 REGLET COLOR: SHERWIN WILLIAMS 6356 COPPER MOUNTAIN  
 REGLET SHEEN : FLAT
2. FIBERCEMENT SIDING PANEL  
 COLOR : SHERWIN WILLIAMS SW 7566 WESTHIGHLAND WHITE  
 SHEEN : FLAT  
 PANEL SHEEN : FLAT  
 REGLET COLOR: SHERWIN WILLIAMS 7566 WESTHIGHLAND WHITE  
 REGLET SHEEN : FLAT
3. HORIZONTAL FIBERCEMENT SIDING  
 COLOR : SHERWIN WILLIAMS SW 7675 SEALSKIN  
 SHEEN : FLAT
4. METAL FLASHING  
 COLOR : SIDING PANEL COLOR
5. BLACKENED SHEET STEEL OVER RAINSCREEN
6. SHEET STEEL SUNSCREEN W/ JULIET BALCONY SEE DETAIL 4/A4.20  
 COLOR : SHERWIN WILLIAMS SW 7675 SEALSKIN  
 SHEEN : FLAT
7. SHEET STEEL SUNSCREEN  
 COLOR : SHERWIN WILLIAMS SW 7675 SEALSKIN  
 SHEEN : FLAT
8. VINYL WINDOW / DOOR ASSEMBLY  
 COLOR : DARK GREY
9. VINYL WINDOW / DOOR ASSEMBLY  
 COLOR : WHITE
10. ALUMINUM CLAD WOOD WINDOW  
 COLOR : IRON ORE
11. EXTERIOR STEEL STAIR  
 COLOR : SW 7675 SEALSKIN  
 SHEEN : SEMI-GLOSS
12. STEEL RAILING W/ McNICHOLS TECHNA 3155 MESH STEEL PANEL  
 SEE DET. 6 AND 7 A4.20  
 RAILING COLOR : SW 7675 SEALSKIN  
 RAIL SHEEN : SEMI-GLOSS  
 MESH PANEL COLOR : NATURAL STEEL
13. STEEL BALCONY  
 COLOR : SW 7675 SEALSKIN  
 SHEEN : SEMI-GLOSS
14. PRE-FABRICATED PLANTER  
 TOURNESOL WILSHIRE COLLECTION
15. CONCRETE RETAINING WALL
16. SELF DRAINING LOUVER W/ 2 1/2" GRILL SPACING  
 COLOR : SHERWIN WILLIAMS SW7566 WESTHIGHLAND WHITE  
 SHEEN : FLAT
17. SEI-HO ENVIRONMENTAL GRILL  
 MODEL: SFB  
 FINISH : ANODIZED ALUMINUM

ALLEY ELEVATION (EAST)




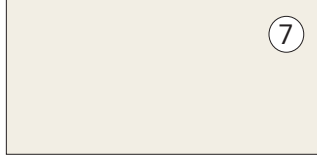
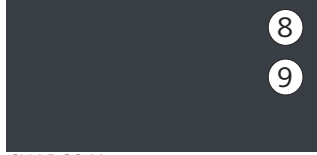


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NORTH ELEVATION

06.13.2013 DESIGN REVIEW RECOMMENDATION MEETING #2  
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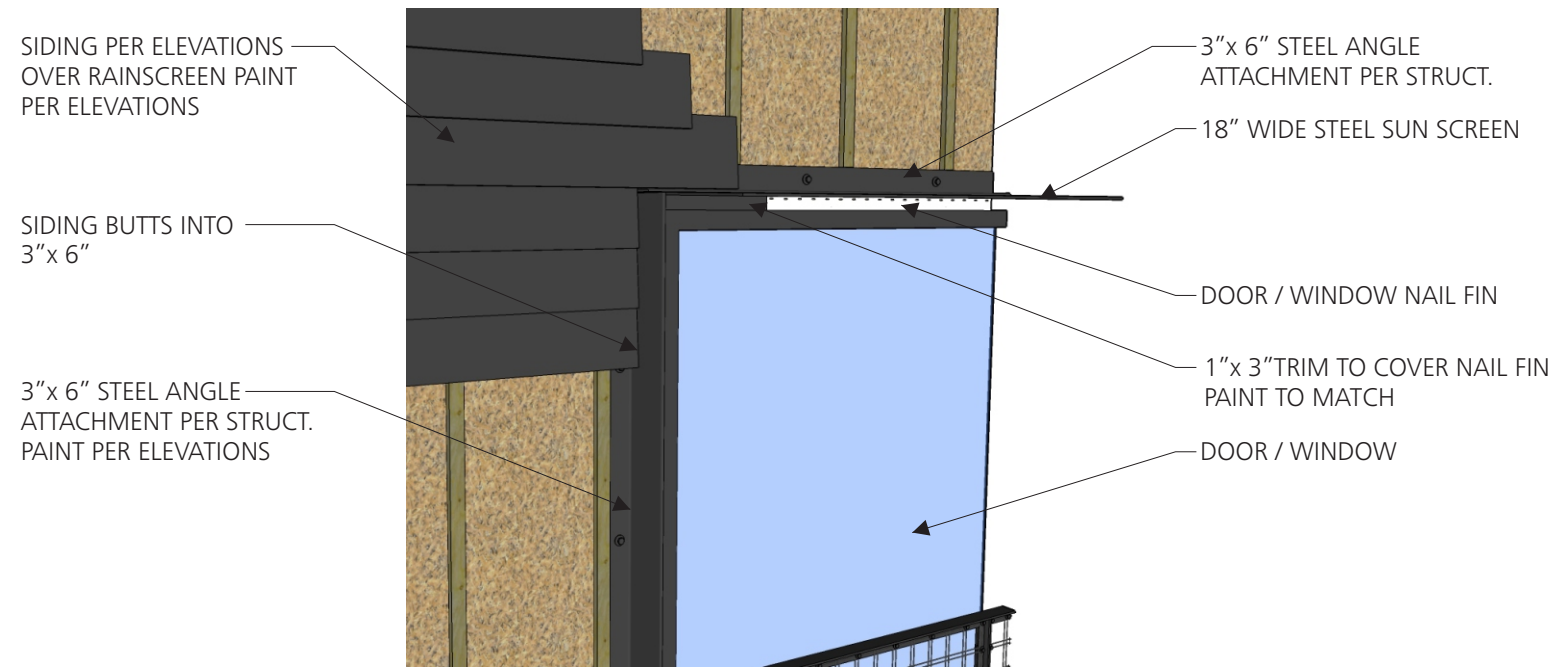
-  ① FIBER-CEMENT PANEL SIDING  
SHERWIN WILLIAMS  
SW 6356 COPPER MOUNTAIN
-  ② FIBER-CEMENT HORIZONTAL SIDING
- ③ STEEL SUNSCREEN
- ④ HANDRAIL
-  ⑤ BLACKENED PLATE STEEL PANEL
- ⑥ PRE-FABRICATED PLANTER
-  ⑦ FIBER-CEMENT PANEL SIDING  
SHERWIN WILLIAMS  
SW 7566 WESTHIGHLAND WHITE
-  ⑧ DOOR/WINDOW (VINYL)
- ⑨ DOOR/WINDOW (STOREFRONT)
-  ⑩ DOOR/WINDOW (VINYL)
- ⑪ CONCRETE PATH
- ⑫ FENCE
-  ⑬ SANDBLASTED CONCRETE



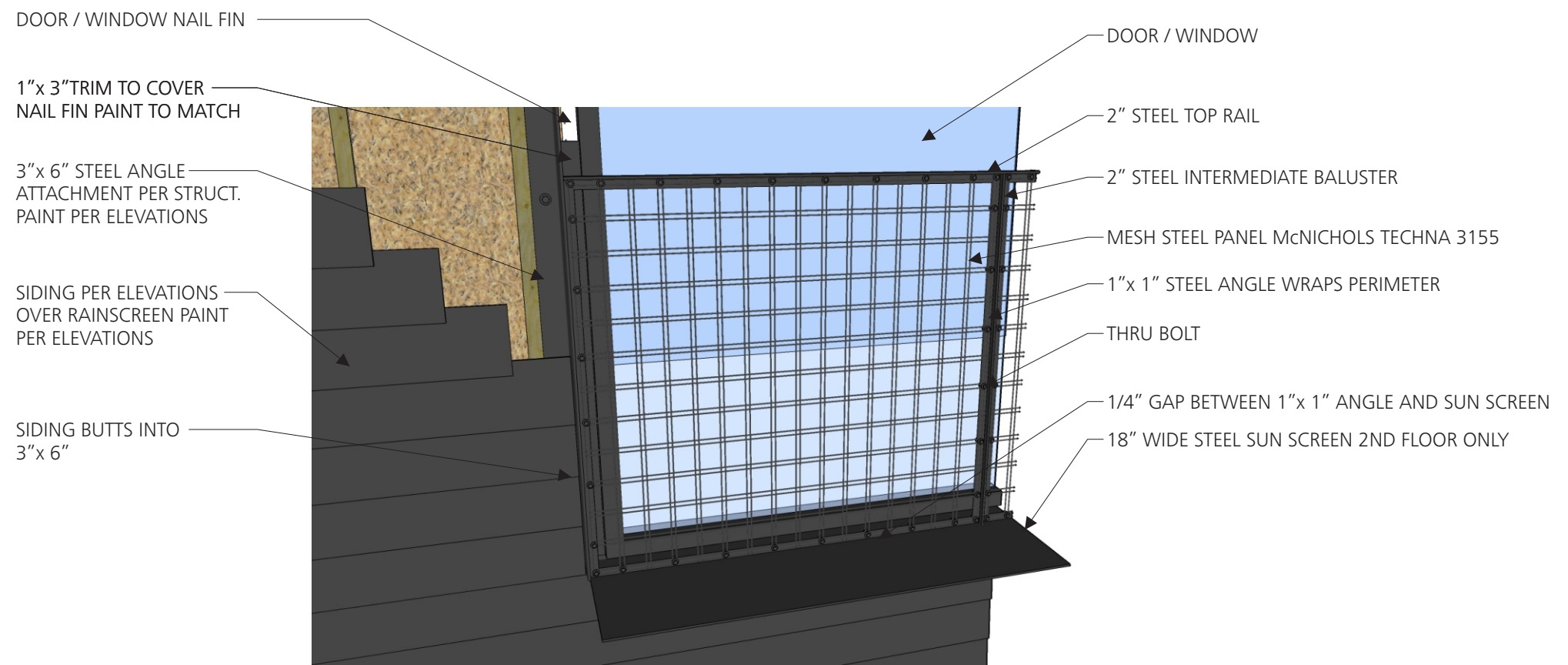
EXTERIOR MATERIALS

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JULIET BALCONY SUN SCREEN DETAIL AT DOOR / WINDOW HEAD



JULIET BALCONY RAILING DETAIL

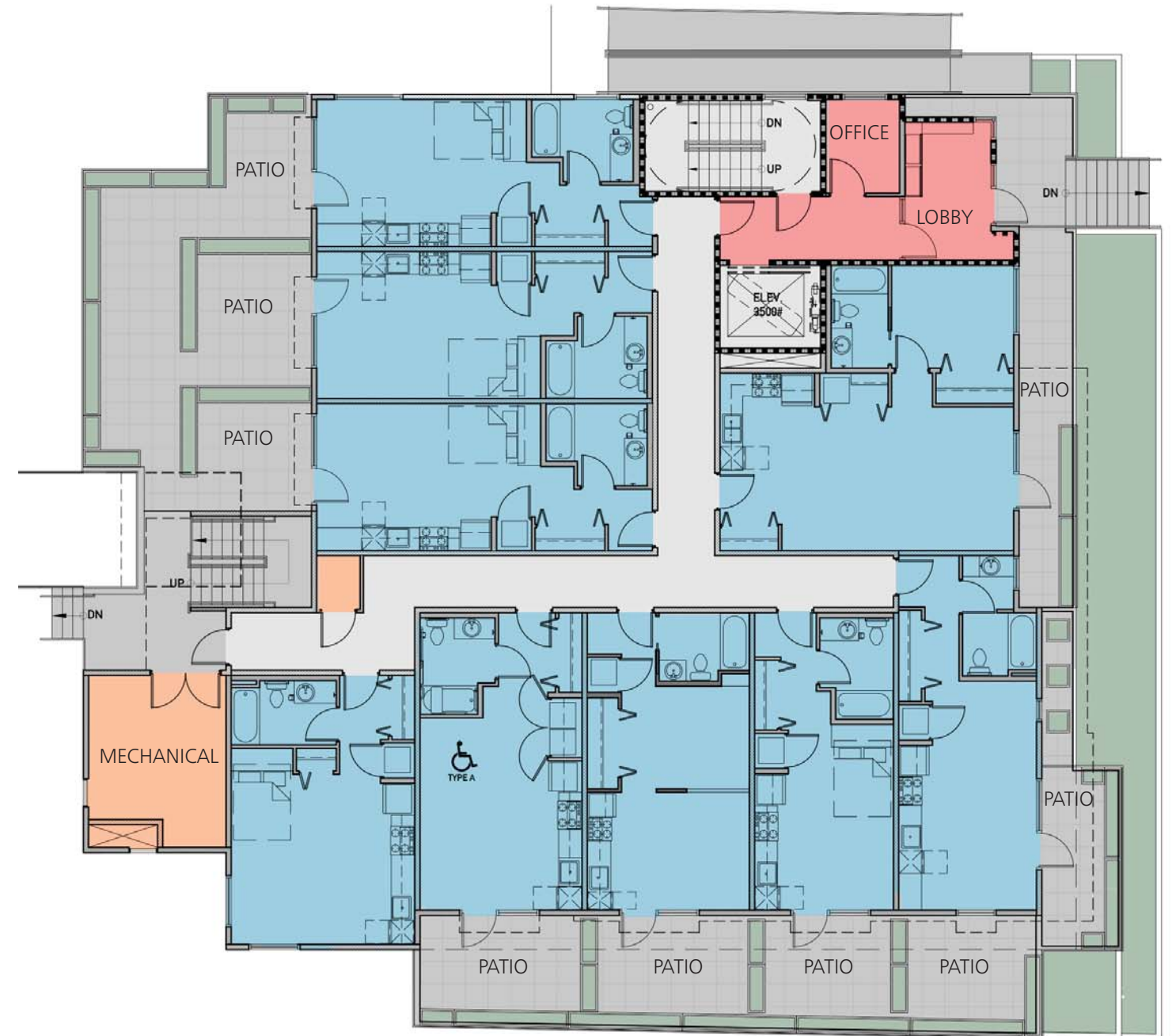
JULIET BALCONY DETAILS

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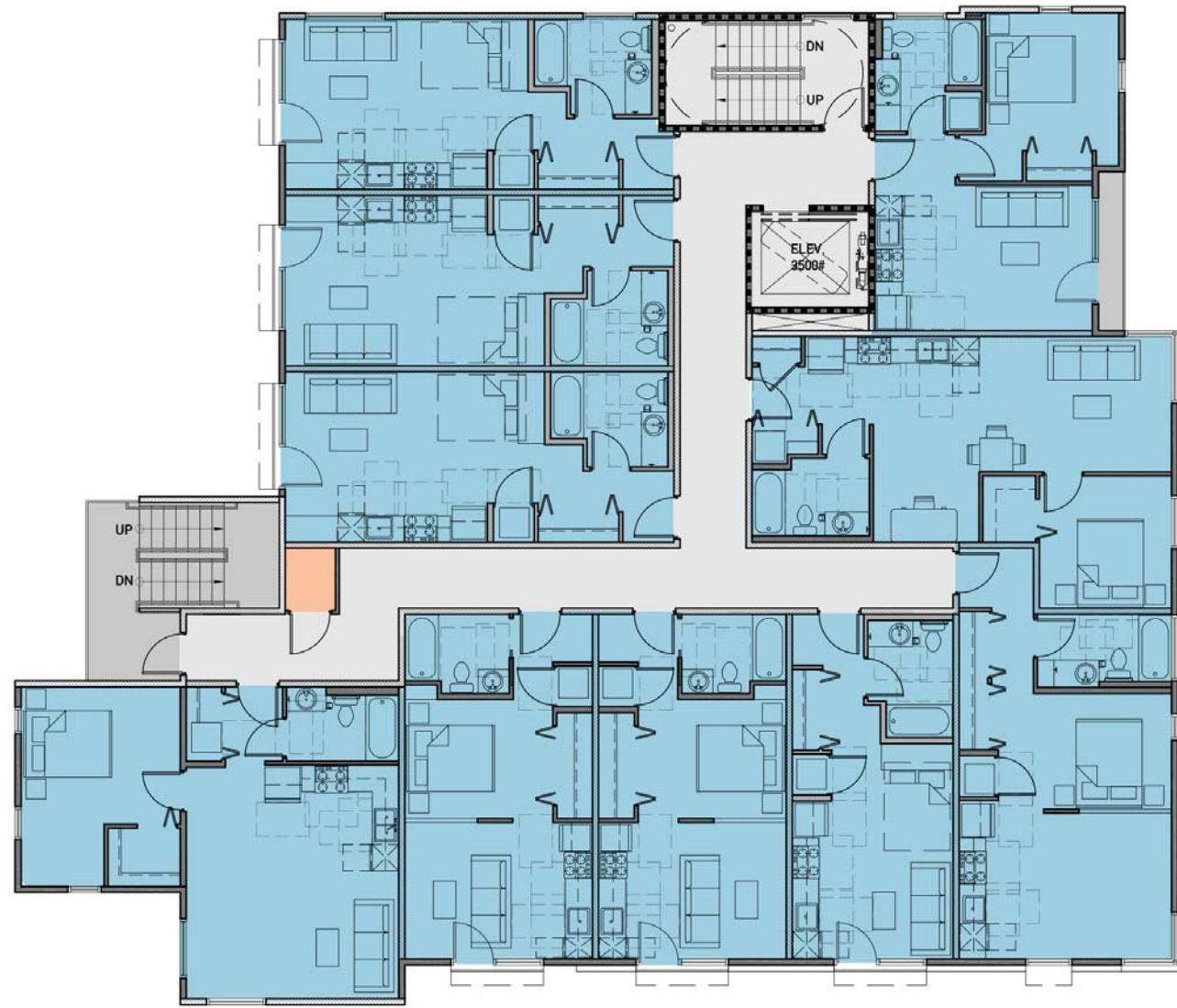
PARKING LEVEL



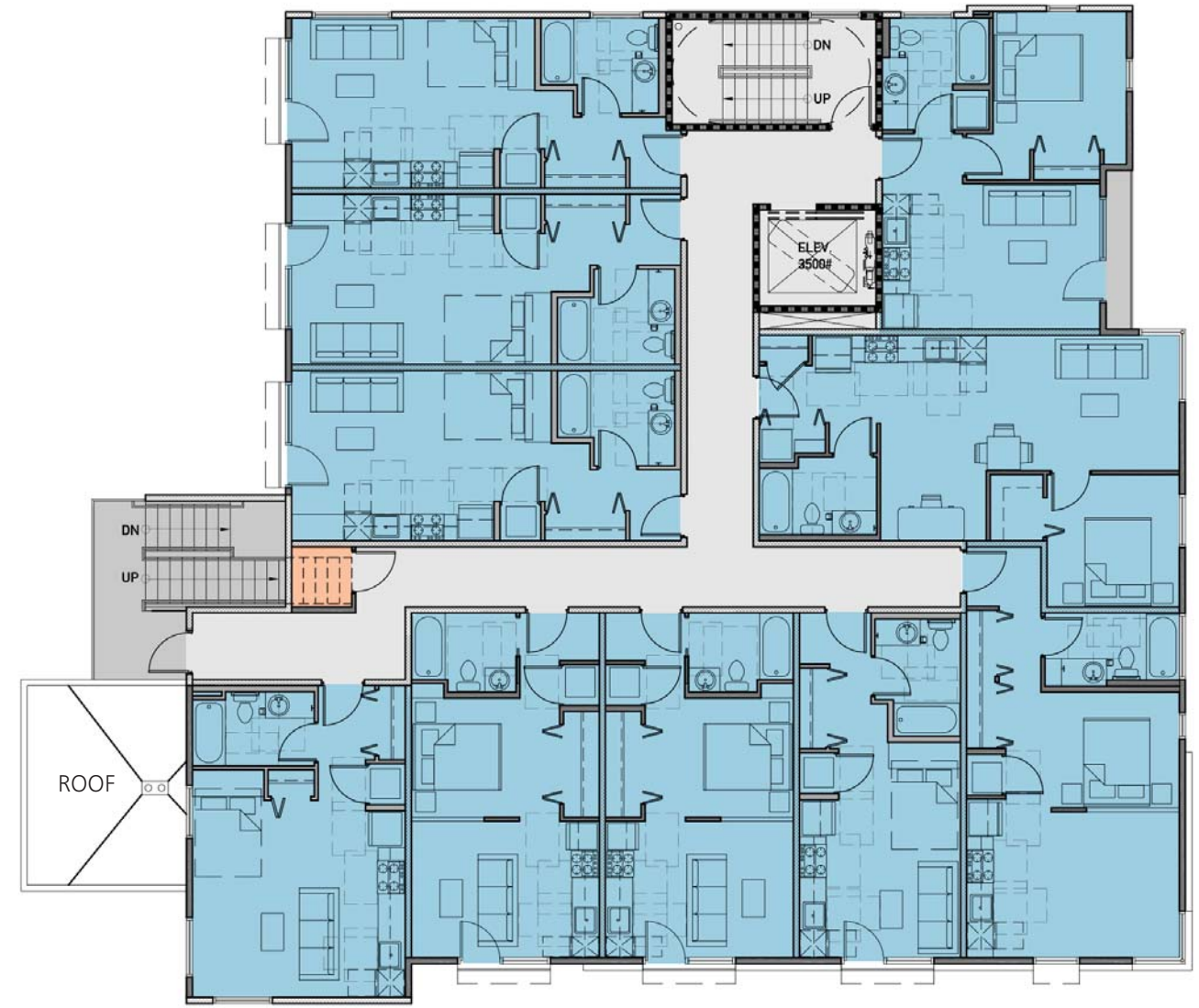
FIRST LEVEL

FLOOR PLANS

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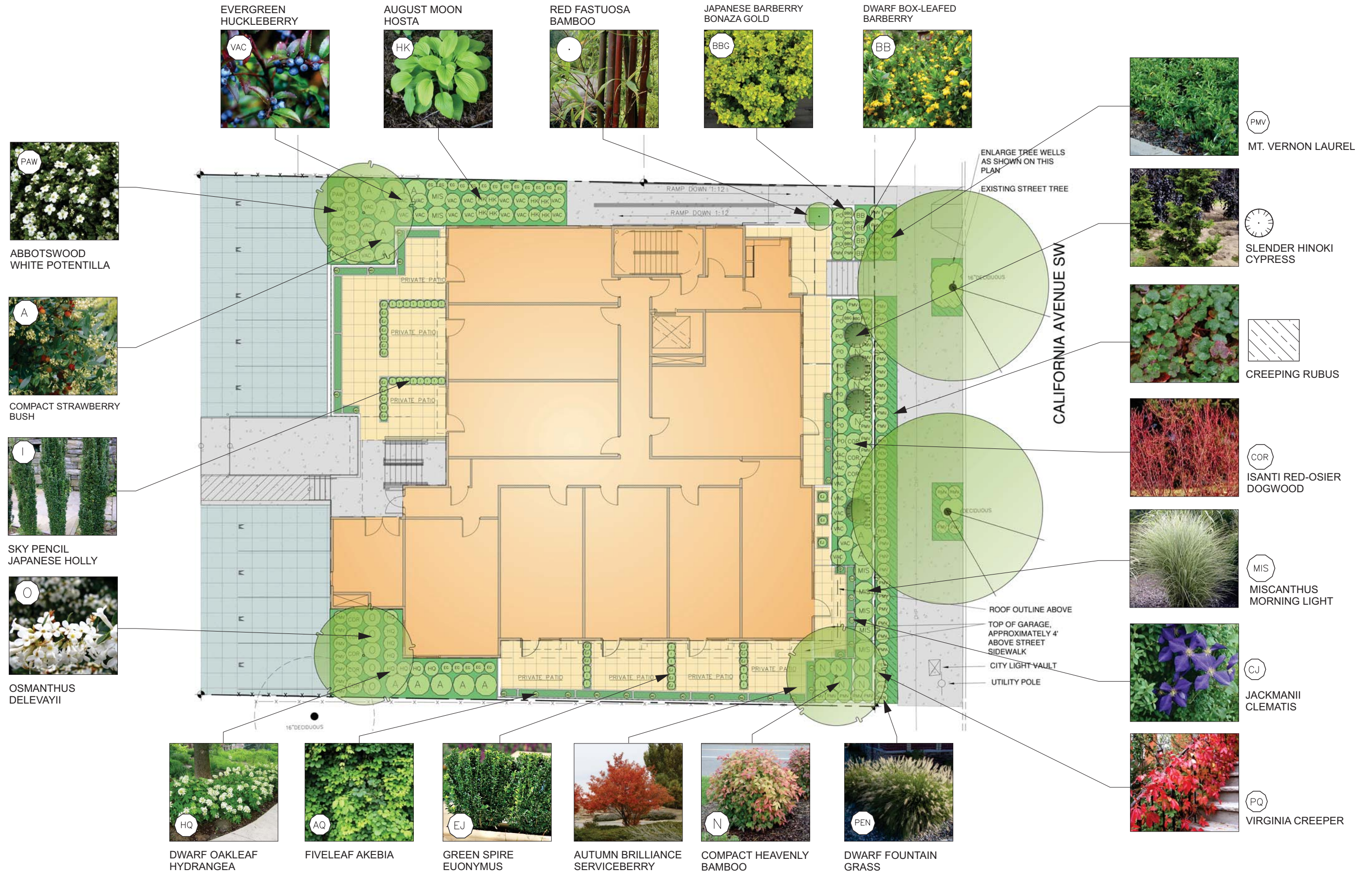
SECOND LEVEL



THIRD LEVEL

FLOOR PLANS

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LANDSCAPE PLAN - GROUND AND FIRST LEVEL PLANTINGS

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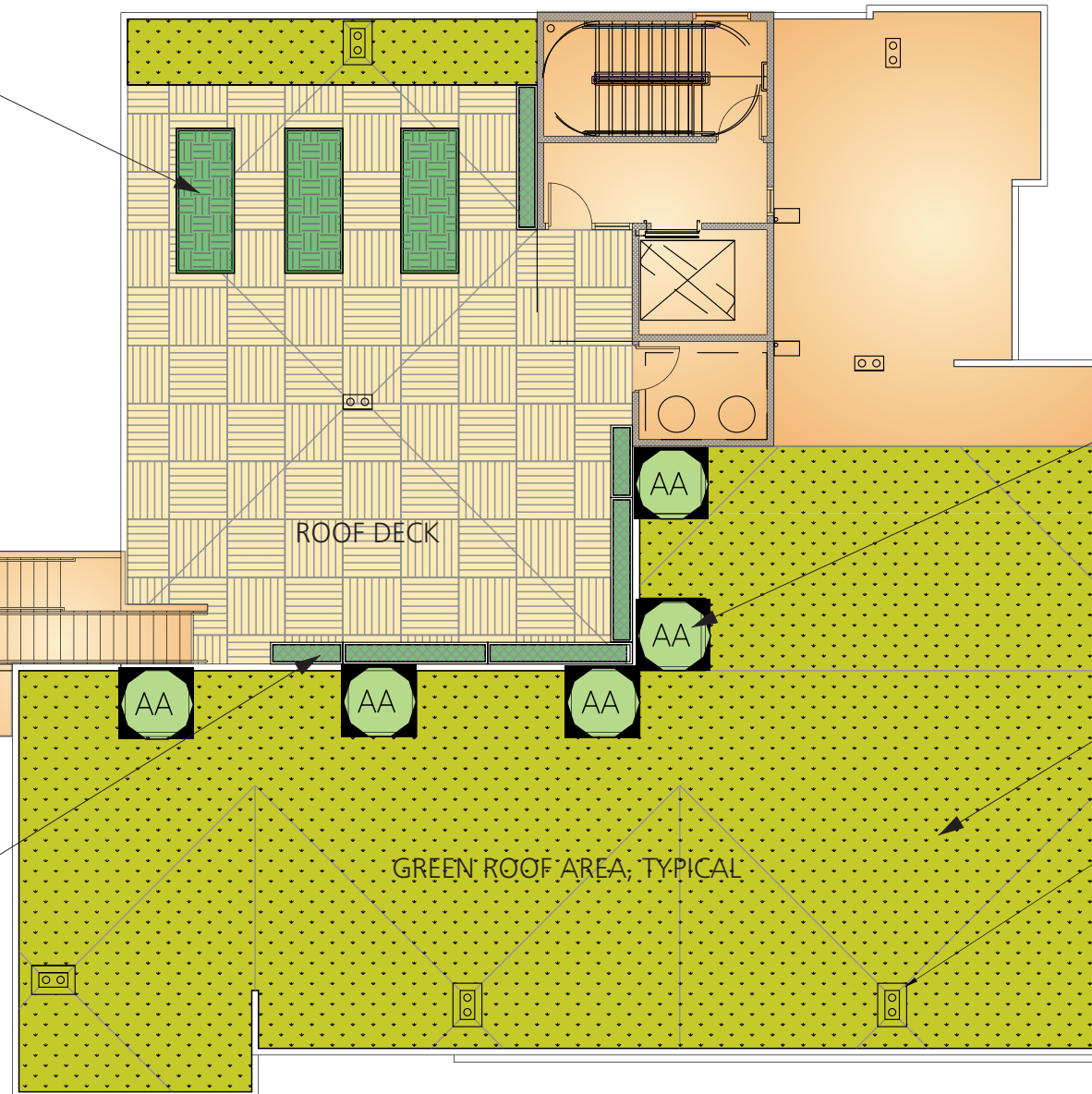


PREFABRICATED PLANTER

P-PATCH PLANTERS  
TYPICAL OF 3

OPEN STAIR

PREFABRICATED PLANTER  
TYPICAL



AA  
BRILLIANT RED  
CHOKEBERRY



AA  
SEDUM AUTUMN JOY

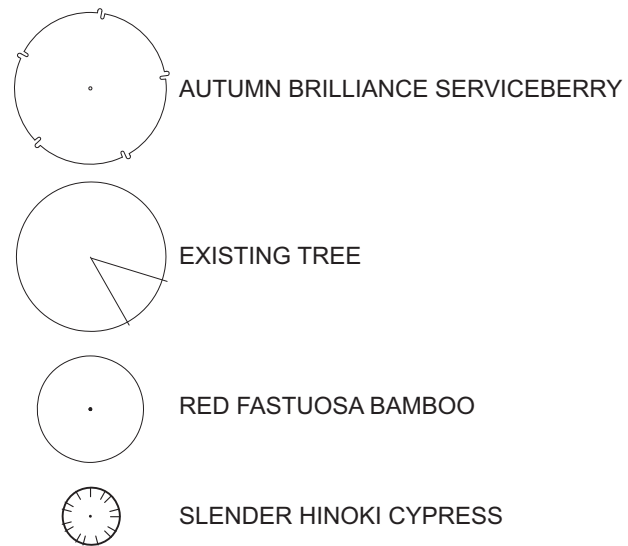
LANDSCAPE PLAN - ROOFTOP PLANTINGS

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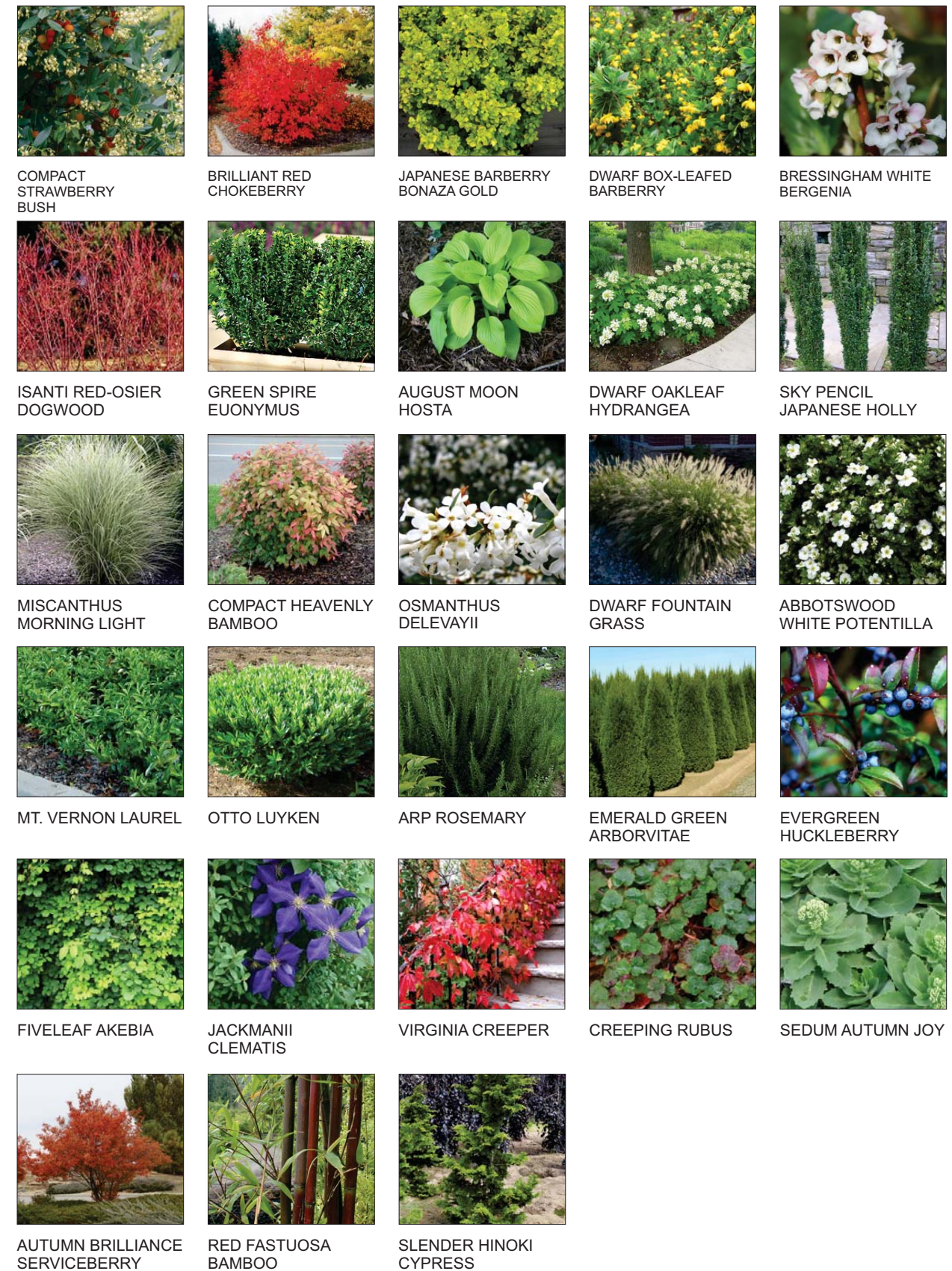


(A)*	COMPACT STRAWBERRY BUSH
(AA)*	BRILLIANT RED CHOKEBERRY, MULTISTEM
(BBG)*	JAPANESE BARBERRY BONANZA GOLD
(BB)*	DWARF BOX-LEAFED BARBERRY
(BBW)*	BRESSINGHAM WHITE BERGENIA
(COR)*	ISANTI RED-OSIER DOGWOOD
(EJ)*	GREEN SPIRE EUONYMUS
(HK)	AUGUST MOON HOSTA
(HQ)*	DWARF OAKLEAF HYDRANGEA
(I)*	SKY PENCIL JAPANESE HOLLY
(MIS)*	MISCANTHUS MORNING LIGHT
(N)*	COMPACT HEAVENLY BAMBOO
(O)*	OSMANTHUS DELEVAYII
(PEN)	DWARF FOUNTAIN GRASS
(PAW)*	ABBOTSWOOD WHITE POTENTILLA
(PMV)	MT. VERNON LAUREL
(PO)*	OTTO LUYKEN LAUREL

(RA)*	ARP ROSEMARY
(EG)*	EMERALD GREEN ARBORVITAE
(VAC)*	EVERGREEN HUCKLEBERRY
(AQ)	FIVELEAF AKEBIA
(CJ)	JACKMANII CLEMATIS
(PQ)	VIRGINIA CREEPER
(Grid)	PLANTER BOX
(Diagonal Lines)	CREEPING RUBUS
(Dotted)	SEEDUMS FOR GREEN ROOF PLANTINGS
(Brick Pattern)	RAISED BED EDIBLE GARDEN

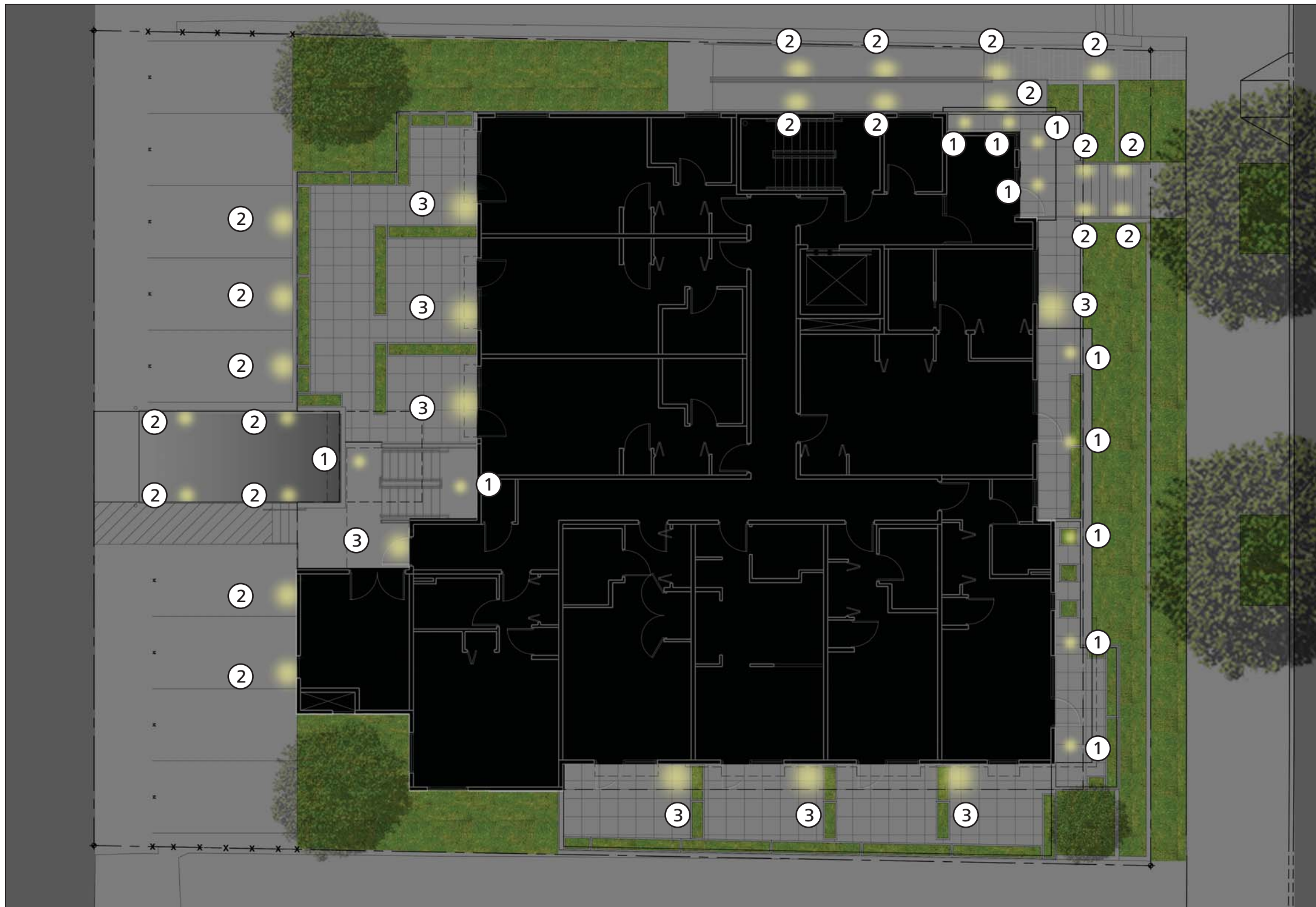


\* SHRUB WITH MATURE HEIGHT OF 24" OR GREATER



LANDSCAPE SCHEDULE

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① RECESSED CEILING LIGHTING  
SLV HORN LED  
RECESSED LUMINAIRE



② RECESSED WALL LIGHT  
SLV DOWNUNDER LED 14  
RECESSED LUMINAIRE



③ EXTERIOR SCONCES  
SLV ASTINA OUT ESL  
EXTERIOR WALL



LIGHTING PLAN

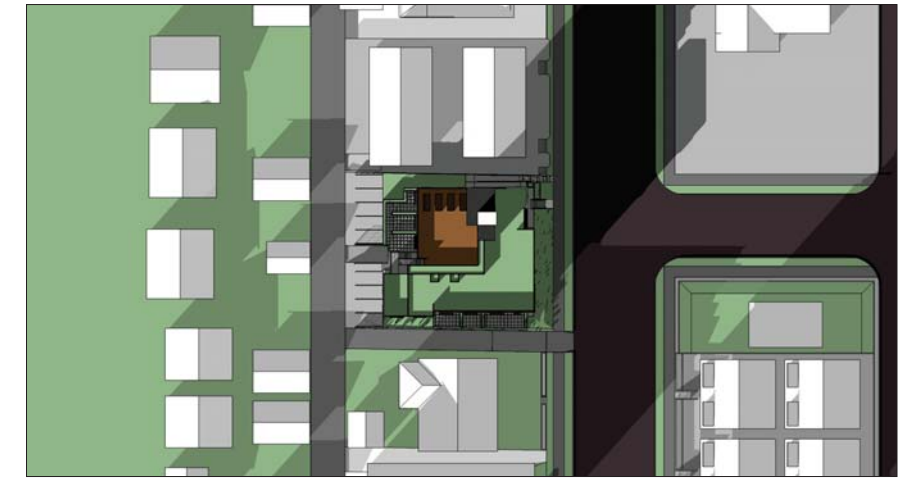
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12/21 9am



12/21 12pm



12/21 3pm



6/21 9am



6/21 12pm



6/21 3pm



3/21 9am



3/21 12pm

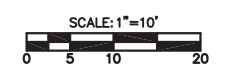
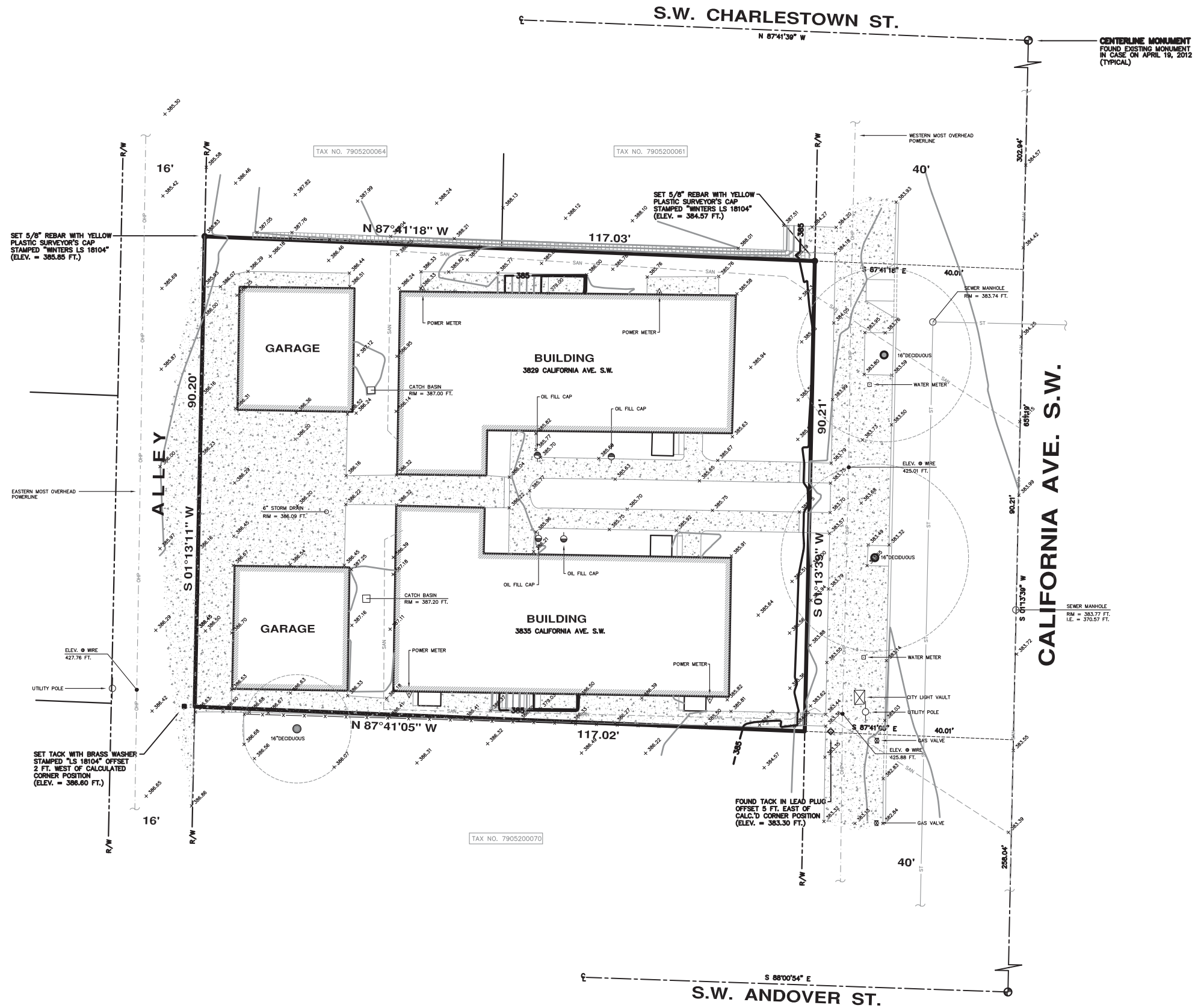


3/21 3pm

SHADOW STUDIES

06.13.2013 DESIGN REVIEW RECOMMENDATION MEETING #2  
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**NOTES**

1. THIS SURVEY WAS PERFORMED BY FIELD TRAVERSE USING A 10 SECOND "TOTAL STATION" THEODOLITE SUPPLEMENTED WITH A 100 FT. STEEL TAPE. THIS SURVEY MEETS OR EXCEEDS THE STANDARDS FOR LAND BOUNDARY SURVEYS AS SET FORTH IN WAC CHAPTER 332-130-090.
2. CONTOUR INTERVAL = 1 FT.
3. ELEVATION DATUM = NAVD'88, ELEVATION 355.66 FT. AS PER BENCH MARK NO. 141 AS SHOWN AND DESCRIBED IN CITY OF SEATTLE 1993-1994 JAMP VERTICAL REPORT
4. PARCEL AREA = 10,554 SQ. FT.
5. THIS SURVEY WAS PREPARED WITHOUT THE BENEFIT OF A CURRENT TITLE REPORT. THEREFORE EASEMENTS AFFECTING THIS PROPERTY, IF ANY, ARE NOT SHOWN HEREON.
6. UNDERGROUND UTILITY INFORMATION AS SHOWN HEREON IS APPROXIMATE ONLY AND IS BASE UPON CITY OF SEATTLE SEWER CARD NO. 3611 AND ALSO AS PER TIES TO ABOVE GROUND STRUCTURES.
7. TAX PARCEL NO. 7905200085

**PROPERTY DESCRIPTION**

LOT 3, EXCEPT THE NORTH 27.5 FT., BLOCK 2, SPARKMAN & MCLEAN'S SECOND ADDITION TO WEST SEATTLE, ACCORDING TO THE PLAT THEREOF RECORDED IN VOLUME 10 OF PLATS, PAGE 99, RECORDS OF KING COUNTY, WA.

SITE SURVEY

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